



Department of Community Services

Transportation Division

August 23, 2024

Jon Henrichsen
County Engineer
1620 SE 190th Avenue
Portland, OR 97233

Subject: Full Road Closure and Work Hour Change Request – SE Lusted Road, SE Carpenter Lane, and SE Hosner Road

Multnomah County Right of Way Permits received a request from Portland Water Bureau on August 9, 2024 to temporarily close four segments of roadway to complete full depth reclamation roadwork (the Fix it First work) to fulfill a requirement within the Hearing's Officer Notice of Decision [Conditions of Approval 6.a-d]. The following segments that will be closed as part of this work are listed below (full details of the extents and schedule of road closures can be found in the attached materials):

- 850-feet of SE Altman Road (between SE Dodge Park Boulevard and SE Carpenter Lane)
- 2,100-feet of SE Altman Road (between SE Carpenter Lane and SE Bluff Road)
- 2,800-feet of SE Lusted Road (between SE Cottrell Road and SE Altman Road)
- 2,600-feet of SE Hosner Road (between SE Lusted Road and SE Oxbow Drive)

Portland Water Bureau also requested an adjustment of work hours for this work, which the County has approved. Work hours have been modified from the original 7:00 am - 6:00 pm Monday - Friday to be 7:00 am - 7:00 pm Monday - Friday. No work shall occur Sunday, September 1, 2024 or Monday, September 2, 2024 (Labor Day).

Right of Way Permits staff has conditionally approved the temporary road closure with the following conditions:

- The applicant/contractor must comply with all other conditions in the "Multnomah County Construction Permit", including the submitted traffic control plan (TCP) approved by staff engineer;
- Access to properties must be maintained for residents, visitors, and emergency services. [Road Rules 13.500]
- The applicant/contractor shall inform emergency services immediately (by end of day Friday, August 23, 2024) and reach out to emergency services on a daily basis to provide any additional updates/address concerns from them given the closures;
- Notify Residents of Road Closure at least 48 hours in advance of the work, as required by the general notes in the approved construction plans; and



Department of Community Services

Transportation Division

- Provide a temporary roadway with an unpaved surface along SE Altman Road between SE Dodge Park Boulevard and SE Lusted Road to provide access from Saturday, August 31 starting at 5:00 pm through Wednesday, September 4, 2024 at 7:00 am.

Approval is requested per authority granted the Director by ORS 203.035, ORS 368.011, and MCC 29.503, MCC 29.583.

Sincerely,

Brad Choi, PE
Transportation Development Manager

Approve _____

Disapprove _____

Date 8/23/2024

cc: Right-of-way Permits office

Tyler Kunter, Road Maintenance Supervisor

Ken Bossen, Road Maintenance Supervisor

Erick Johnson, Road Maintenance Supervisor

Lieutenant Doug Asboe, Multnomah County Sheriff

Lieutenant Carey Kaer, Multnomah County Sheriff

Sarah Hurwitz, Multnomah County – Public Affairs Office

August 23, 2024

Multnomah County
Transportation Division
1620 SE 190th Avenue
Portland, OR 97233-5910

RE: Bull Run Filtration Project - Fix-it-First Road Closures

This memo provides an update to the August 16th Road Closures Memo submitted to County Transportation for the Portland Water Bureau's Fix-it-First right-of-way improvements under Permit # ROW-CONST-2024-0010 for the Filtration and Pipelines project. After further discussion with the contractor, PWB proposes the following updated road closure schedule to ensure this road work can be completed before school begins on September 3rd and with minimal disruption to the community. The closures are scheduled to begin Monday August 26th and will last through September 4, 2024 with some exceptions as described in the schedule below. Additionally, PWB requests to revise the work hours for the road work to Monday through Friday 7 am to 7 pm to ensure the proposed roadwork is completed on time. This is a one-hour deviation from the 7 am to 6 pm work hours currently provided on the approved construction plans.

The scope of work within Multnomah County right-of-way includes rehabilitation of pavement along SE Altman Rd between SE Lusted Rd and the County Line; SE Lusted Rd from Beaver Creek to SE Cottrell Rd; and SE Hosner Rd between SE Lusted Rd and SE Oxbow Dr (see Figure 1 below). To perform this work, the following road closures are proposed to expedite the work and avoid as much as possible any conflicts with the start of school on September 3, 2024.

Fix-it-First Road Closure Schedule (see Figure 1)

- Lusted Road (Cottrell Road to east of Altman Road intersection)**
 - 8/26-27 closed* 7am-7pm
- Altman Road (Dodge Park Boulevard to county line)**
 - 8/26 closed* 7am-7pm
 - 8/27-29; closed* all day and night
 - 8/30 closed* 7am-7pm
- Hosner Road (Oxbow Drive to Lusted Road)**
 - 8/28-29 closed* 7a-7p
- Altman Road (Lusted Road to Dodge Park Boulevard)**
 - 8/28 closed* 7a-7p
 - 8/29-8/31 closed* until 8/31 5 pm
 - 8/31 5 pm - 9/4 7 am unpaved (sand) road open; 15 mph speed limit

- 9/4 closed* for paving 7 am-7pm

Figure 1 – Fix-it-First Road Closures



**Local and emergency access only during road closures. For detours and updates, visit www.portland.gov/FiltrationConstruction*

Multnomah County Road Rules (MCRR)

MCRR 13.450 states that “requests for closure of twenty-four (24) hours or more must be submitted at least 14 days before the proposed closure. Public notice will be required for closures of more than twenty-four (24) hours.” PWB has shared the request for the Fix-it-First road work with County Transportation in bi-weekly meetings, PWB’s permit application submitted on July 1, 2024 for this work included traffic control plans showing full closures. PWB also submitted a project schedule on 8/9 to County staff with proposed closure dates.

PWB canvassed property owners on August 2, 2024 along the project route and shared maps of the work areas and a general description of the work. Additionally, information about the Fix-it-First work has been shared in PWB’s weekly construction update to the public (including Emergency Responders), updated weekly on its project website, and was shared with the Gresham-Barlow School District’s Student Safety Committee via email on August 1, 2024 and in a meeting on August 7, 2024. PWB canvassed property owners along the project route on 8/23/24 with the detailed project schedule and closure details. PWB believes it has demonstrated compliance with MCRR

13.450 based on this information. An email under separate cover sent to County Transportation on August 20, 2024, includes documentation to support this outreach.

PWB appreciates the County's evaluation of the proposed closures and the consideration of extended work hours. If you have any questions, please do not hesitate to call me at 503-319-9207 or email Robert.Fraley@portlandoregon.gov.

Sincerely,

Robert Fraley
Principal Program Analyst
Portland Water Bureau

MULTNOMAH COUNTY DEPARTMENT OF COMMUNITY SERVICES
TRANSPORTATION PLANNING AND DEVELOPMENT PROGRAM

Construction Permit

AUGUST 12, 2024

Under the provisions of Multnomah County Code (“MCC”) Sections 27.055, 27.056, 29.506, and 29.572, and Multnomah County Road Rules (“MCRR”) 18.200, this Construction Permit (the “Permit”) is hereby issued by Multnomah County Transportation Planning and Development Program (“County”) to:

PORTLAND WATER BUREAU

1120 SW 5TH AVE, RM 405, PORTLAND, OR 97204

CONTACT: ROBERT FRALEY

CONTACT PHONE: 503-823-3573

CONTACT EMAIL: Robert.fraley@portlandoregon.gov

(“Developer”). This Permit is issued for the construction of the improvements within the County controlled rights-of-way or easements, as described in EXHIBIT A, attached hereto and made a part of this document.

A. General Permit Requirements:

1. The County shall approve all plans, drawings, and other design documents prior to the construction of any improvements as described in the Exhibit A.
2. NO PARTIAL IMPROVEMENTS SHALL BE CONSTRUCTED WITHIN THE COUNTY RIGHT-OF-WAY.
3. ANY EXCAVATION, ROCKING, OR PAVING SHALL ONLY BE ALLOWED AS A PART OF A CONTINUOUS, TIMELY OPERATION TOWARD COMPLETION OF THE WORK AS DESCRIBED IN THE EXHIBIT “A.” NO PAVING SHALL BE ALLOWED UNTIL ALL CURB, EXCAVATION, ROCK, DRAINAGE, AND UTILITY WORK, HAS BEEN COMPLETED WITHIN THE AREA TO BE PAVED, AND THEN ONLY AS A PART OF THE CONTINUOUS, TIMELY COMPLETION OF THE ENTIRE PAVING PROJECT, AS SHOWN ON THE APPROVED PLANS.
4. The County reserves the right to prohibit any further connections, withhold occupancy permits, or remove or block any existing connections between the facilities covered by this Permit and adjacent properties, if the Developer does not provide or complete all items of this Permit to the County's satisfaction.

B. The County agrees to do the following:

1. Establish the amounts of the Administration Fee, Performance Guarantee, and Maintenance Guarantee.
2. Review plans, drawings, and other design documents for conformance with design and drafting criteria. Upon approval of the plans, provide the Developer with copy sets of plans and specifications, and schedule a pre-construction meeting.
3. Establish a reasonable length of time for construction activities within the right-of-way. Issue "First Notification" to authorize the beginning of construction activities, and start the calendar day count
4. Issue "Second Notification" to stop the calendar day count if the County deemed the project to be substantially complete in accordance to the contract documents so that the work can be used for its intended purpose.
5. Perform all required final construction supervision, inspection, and testing if needed.
6. Inspect completed facility for acceptance of construction work, and if facilities are accepted issue the "Third Notification" to release the Performance Guarantee.
7. Conduct final inspection prior to expiration of Maintenance Guarantee, and if facilities are accepted issue the "Fourth Notification" to release the Maintenance Guarantee.
8. Administration required for processing and accepting facility as a County or Service District maintained facility, where applicable.

C. The Developer agrees to do the following:

1. Deposit with the County an Administration Fee, of **\$ See Exhibit A** prior to scheduling the pre-design meeting. Following acceptance by the County of the completed improvements, this fee may be adjusted up or down to reflect the actual costs incurred by the County. Over-payment will be refunded to the Developer. Under-payment will result in billing the Developer for the difference between the actual costs incurred and the initial fee.
2. Before issuance of the "First Notification" provide to the County a properly executed Performance Guarantee in the amount of **\$ See Exhibit A**. This guarantee shall be submitted to the County Engineer. The Performance Guarantee shall guarantee the completion of the improvements described in Exhibit A and shall remain in full force and effect until the Developer receives the "Third Notification" or termination of this Construction Permit whichever comes first.
3. Before issuance of the "Third Notification" provide to the County a properly executed Maintenance Guarantee, in the amount of **\$ See Exhibit A**. The guarantee shall be submitted to the County Engineer. The guarantee shall not expire for two (2) years from date Developer receives the "Third Notification" or receives the "Fourth Notification", whichever comes later. The Maintenance Guarantee shall guarantee the repair or replacement of any improvements constructed under this permit which become

unsatisfactory to the County because of workmanship or materials used during their construction or that are damaged by project workmen or equipment completing the related adjacent construction.

4. Complete all construction activities, to include all "As-Built" information, within eighteen (18) months of the issuance date of the "Third Notification".
 5. Obtain the services of a consulting Engineer, registered in the state of Oregon, WHO WILL BE FURNISHED A COPY OF THIS PERMIT BY THE DEVELOPER. The Developer or Consultant shall furnish to the County a fully executed "Consulting Engineer's Statement" (see attachment), AND SCHEDULE A PRE-DESIGN MEETING WITH THE COUNTY PRIOR TO COMMENCING ANY DESIGN WORK. If Developer should change consulting engineers prior to acceptance of plans a new fully executed "Consulting Engineer's Statement" must be furnished to the County before the project can continue. Any compensation paid to the Engineer for services rendered in performance of Developer's obligations under this Permit may not be included as part of the Administration Fee.
 6. Convey all right-of-way dedications and easement dedications as required by this Construction Permit to the appropriate governing body, free of all encumbrances, before issuance of the "First Notification."
 7. Developer shall defend, save, hold harmless, and indemnify County and its officers, employees, and agents from and against all claims, suits, actions, losses, damages, liabilities, costs, and expenses of any nature whatsoever, including attorney fees, resulting from, arising out of, or relating to the activities of Developer or its officers, employees, agents, contractors, subcontractors under this Permit. Developer shall have control of the defense and settlement of any claim that is subject to this paragraph. However, neither the Developer nor any attorney engaged by the Developer shall defend the claim in the name of the County or any department of the County, nor purport to act as legal representative of the County or any of its departments without receiving from the Multnomah County Attorney's Office, authority to act as legal counsel for the County, nor shall Developer settle any claim on behalf of the County without the approval of the Multnomah County Attorney's Office. County may, at its election and expense, assume its own defense and settlement.
- D. Developer shall ensure that any Consulting Engineer ("Engineer") retained by the Developer to provide services under this Permit complies with the following requirements:
1. The Engineer must work directly with the County Engineer, and provide engineering services in the same manner as if the Engineer had been retained by the County.
 2. Design the project, including complete plans and specifications, in conformance with MCC 29.500, the Multnomah County Design and Construction Manual, and design standards adopted thereunder. All plans submitted for review shall be submitted on 24 inch by 36 inch sheets. Plans shall be produced using English units and quantities.
 3. Perform or obtain all survey work according to County standards, as required for design and construction of the project. Perform any additional construction staking or material testing as directed by the County. The Engineer shall document any changes to the approved design that occur during construction, and submit any additional work required for preparation of

"as-built" plans.

4. After production of "as-built" plans, the Engineer shall provide to the County an electronic copy of the drawings, to include the base topographic survey data, with the "as-built" mylars.
 5. Detail on the plans items of esthetic or historic significance, including trees, inside the right-of-way or within ten (10) feet of the right-of-way line that may be disturbed or destroyed by the project.
 6. Detail on the plans all underground utilities to be constructed as part of this Permit as a composite utility plan. All information is to be furnished by the respective utilities, and approved by them.
 7. Obtain DEQ and appropriate city approval of sanitary sewers, if applicable, and furnish copy of approval to the County prior to the County's approval of plans.
 8. The Engineer shall make all changes to the plans as may be required for County approval of the plans. After all changes have been made, the Engineer shall submit a stamped and signed plans either electronically or on double mat mylar or photocopied mylars on double mat mylars and one set of 11 inch by 17 inch paper. If mylars are hand produced or pen plotted, mylars are to be produced in black ink.
- E. Developer shall ensure that any Construction Contractor ("Contractor") retained by the Developer to provide services under this Permit complies with the following requirements:
1. Before the pre-construction meeting, Contractor shall notify all affected utilities and coordinate their work within the project.
 2. At the time of the pre-construction meeting, the Contractor shall provide to the County the following documents:
 - Traffic control plans
 - Insurance Certificate with Multnomah County as additional insured
 - Construction work schedule
 - List of contractors' contact numbers for emergencies
 - List of subcontractors
 - List of material suppliers
 - Gradation, Abrasion, Sand Equivalent, Fracture, and Degradation Tests for Aggregate
 - Mix Design for Concrete and Asphalt
 - Material Certifications
- All documents shall conform to the requirements of the current ODOT Oregon Standard Specifications for Construction.
3. All activities, including work to be performed and materials to be used in the performance of work as described in the Exhibit/s, shall conform to the current ODOT Oregon Standard Specifications for Construction except as modified by Multnomah County Special Provisions.
 4. Maintain all erosion control measures at all times.

5. Complete all construction activities within the right-of-way within the period specified in the “First Notification” and complete any corrective work as detailed in the “Second Notification” within twenty (20) calendar days from the date of the “Second Notification”. Failure to meet these deadlines shall be grounds for the County to proceed under Section F (2) of this Permit.

F. Time Extension/Forfeiture

1. The County may, in its sole discretion, grant time extensions to the time limits described in this Permit. Any request for a time extension shall be in writing, and state the reasons for the extension. An approved extension may require an additional Administration Fee and may also require a new Performance Guarantee in an increased amount necessary to cover current estimated construction costs. Before the County grants any time extension, the Developer shall furnish proof that all Fees and Guarantees have met the requirements of Sections C (1) and C (2) for the new date being requested.
2. The County may, in its sole discretion, require forfeiture of the Performance Guarantee and complete the project for any of the following:
 - a) Failure to complete construction by the date established in Section C (4)
 - b) Failure to complete construction activities within the time limit established in the “First Notification.”
 - c) Failure to complete the corrective work within 20 calendar days of the date of the “Second Notification.”
 - d) Failure to abide by the other terms of this Permit.
3. If the Performance Guarantee is insufficient to reimburse the County for its costs to complete the project under paragraph 2 of this section, the County shall bill the Developer for any excess costs.

I (We) have read and hereby accept the terms of this Construction Permit.

By David Peters
Developer

12 Aug 2024
(Date)

David Peters, PE
Developer (Print Name)

Tax ID No.

Accepted for Multnomah County, Oregon

By _____
Brad Choi, PE
Transportation Development Manager

8/12/24
(Date)

By _____
Jon Henrichsen, PE
Transportation Division Director

8/12/24
(Date)

Construction Permit No: ROW-CONST-2023-0003.3

Effective Date: 23AUG2024

EXHIBIT "A"

To make frontage/road improvements as part of the Notice of Decision document T3-2022-16220 Hearings Officer's Decision and fulfills the following Conditions of Approval D.6.a, D.6.b, D.6.c, D.6.d, and D.6.i

The improvements are to consist of the following:

1. Road improvements as shown in the attached Construction Plan.

Furnish Administration Fee in amount of \$ 198,000.00 (please see Section C (1) of Construction Permit). Administration Fee must accompany signed Project Permit. If writing a check, please make check payable to: MULTNOMAH COUNTY.

Furnish Performance Guarantee in amount of \$ 2,178,000.00 (please see Section C (2) of Construction Permit).

Furnish Maintenance Guarantee in amount of \$ 435,600.00 (please see Section C (3) of Construction Permit).

Both the Performance Guarantee and the Maintenance Guarantee can be provided in the form of an indemnity agreement between the City and County in a form approved by the County Attorney.

Altman - Hosner - Lusted Roads Pavement Rehabilitation Project

General Notes

- ATTENTION: Oregon law requires you to follow rules adopted by the Oregon Utility Notification Center. Those rules are set forth in OAR 952-001-0010 through OAR 952-001-0090. You may obtain copies of the rules by calling the Center. The Center business number is 503-232-1987. Excavators are required to notify the Center at least 2 business days, but not more than 10 business days, before commencing an excavation. To comply, call 811.
- All elevations are in reference to the City of Portland Datum. To convert from NGVD29(1947) datum to the City of Portland datum, add 1.375 feet. To Convert from NAVD88 to City of Portland datum, subtract 2.10 feet.
- Contractor to provide "As Constructed" drawings indicating all changes in grade, alignment, fittings and materials installed or any other utilities or obstacles not indicated on these plans.
- Locations of existing utilities are based on best available information supplied by the utilities and shall be considered approximate only. As required by state law, the contractor shall obtain utility locates prior to commencing construction.
- Do not remove any trees without approval from Owners Representative.
- All disturbed and cut areas shall be restored by covering with seed mix per Specifications Section 01030.
- All occurrences of "Left" or "Right" on these plans, refer to the direction from centerline, looking up station.
- Except where otherwise indicated in plans, pave to existing edge of pavement.
- All utilities and culverts to be protected.

- Striping details refer to ODOT TM500, TM503 and TM530.
- Asphalt Roadway See Paving Details, Sheet 9.
- Pavement Transitions:
 - New asphalt to existing roadway - ODOT RD610 or as noted.
 - New asphalt to shoulder - ODOT RD615 or as noted.
 - New asphalt to paved approach - Detail 3 Sheet 9, Detail 3 Sheet 10 or as noted
 - New asphalt to gravel approach - Detail 4 Sheet 9, Detail 4 Sheet 10 or as noted.

**MULTNOMAH COUNTY
TRANSPORTATION DIVISION**

ACCEPTED
 ACCEPTED WITH COMMENTS
 APPROVED
 APPROVED AS NOTED
 RETURNED FOR CORRECTION

DOCUMENTS BEARING THIS STAMP ARE SUBJECT TO THE PROVISIONS SET FORTH IN SECTION 00150 OF THE OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION THAT ARE ACTIVE FOR THIS PROJECT. REVIEW IS ONLY FOR GENERAL CONFORMANCE AND COMPLIANCE OF THE PROJECT AND DOES NOT EXTEND TO MEANS, METHODS, TECHNIQUES, SEQUENCES, OR PROCEDURES OF CONSTRUCTION. CONTRACTOR IS RESPONSIBLE FOR COMPLIANCE WITH CONTRACT DOCUMENTS. DIMENSIONS AND FABRICATION TO BE CONFIRMED AND COMPLETED AT THE JOB SITE FOR PROGRESS AND COORDINATION OF THE WORK OF ALL TRADES AND SATISFACTORY WORK PERFORMANCE.

BY Adam Donovan DATE 8/19/2024

List of Drawings

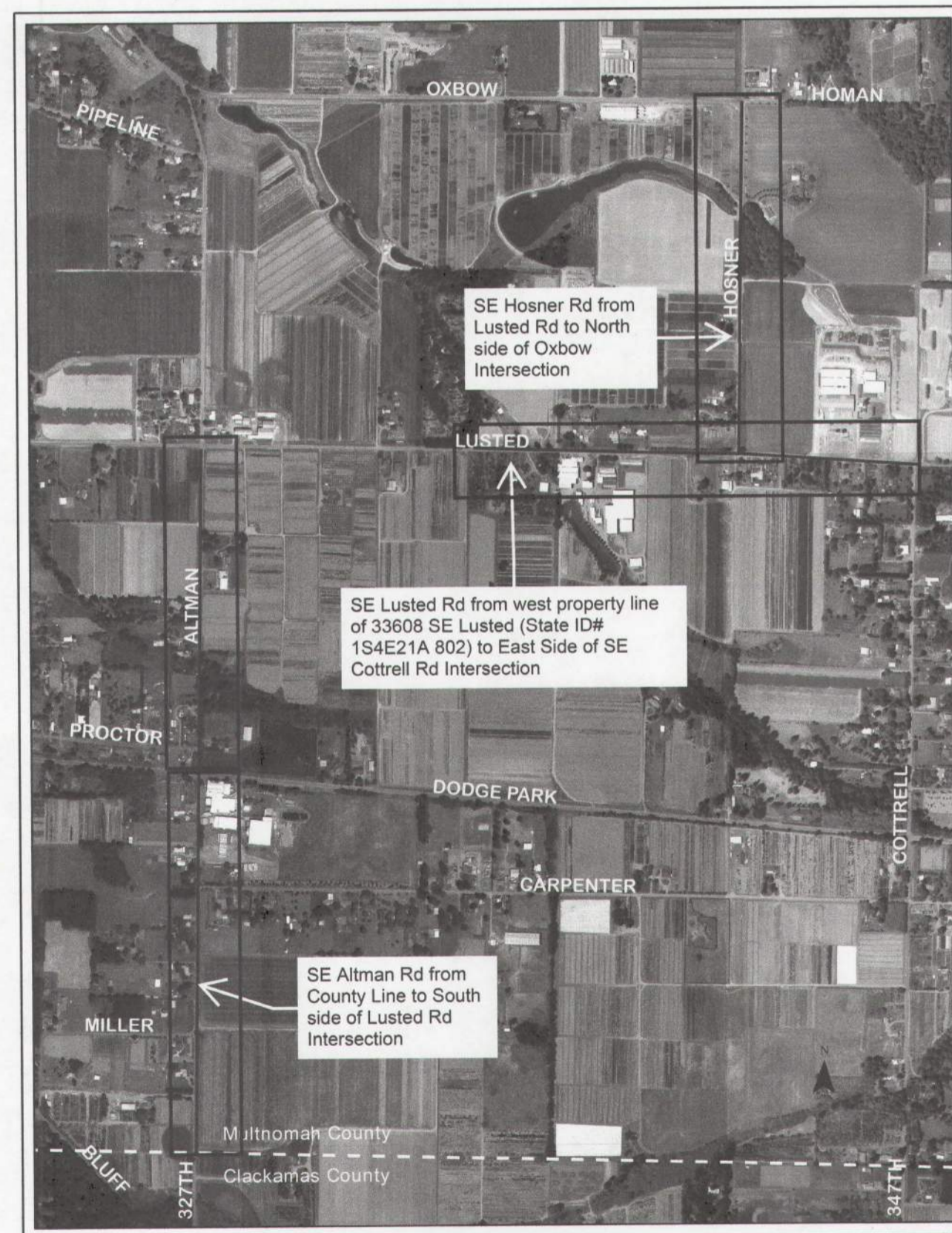
Title Sheet	1
General Notes Sheet	2
Erosion Control Standard Details Sheet	3
Traffic Control Sheet: Land Use Traffic Control Plan Conditions	4
Traffic Control Sheet: SE Altman Rd - South of Dodge Park Blvd	5
Traffic Control Sheet: SE Altman Rd - North of Dodge Park Blvd	6
Traffic Control Sheet: SE Lusted Rd - West of SE Cottrell Rd	7
Traffic Control Sheet: SE Hosner Rd - North from SE Lusted Rd	8
Paving Details Sheet: SE Altman Rd	9
Paving Details Sheet: SE Lusted Rd / SE Hosner Rd	10
ODOT Standard Drawings Detail Sheet: RD610, RD615 & RD715	11
ODOT Standard Drawings Detail Sheet: TM500, TM503 & TM530	12
Blank Sheet	13
SE Altman Rd - Roadway Plan Sheet Sta 0+00A to 10+00A	14
SE Altman Rd - Roadway Plan Sheet Sta 10+00A to 21+00A	15
SE Altman Rd - Roadway Plan Sheet Sta 21+00A to 32+02A	16
SE Altman Rd - Roadway Plan Sheet Sta 32+02A to 43+02A	17
SE Altman Rd - Roadway Plan Sheet Sta 43+02A to 52+35A	18
SE Lusted Rd Plan Sheet Sta 0+00B to 11+90B	19
SE Lusted Rd Plan Sheet Sta 11+90B to 22+36B / SE Hosner Rd 0+00C to 0+82C	20
SE Lusted Rd Plan Sheet Sta 22+36B to 33+01B	21
SE Hosner Rd Plan Sheet Sta 0+82C to 11+36C	22
SE Hosner Rd Plan Sheet Sta 11+36C to 21+86C	23
SE Hosner Rd Plan Sheet Sta 21+86C to 26+13C	24



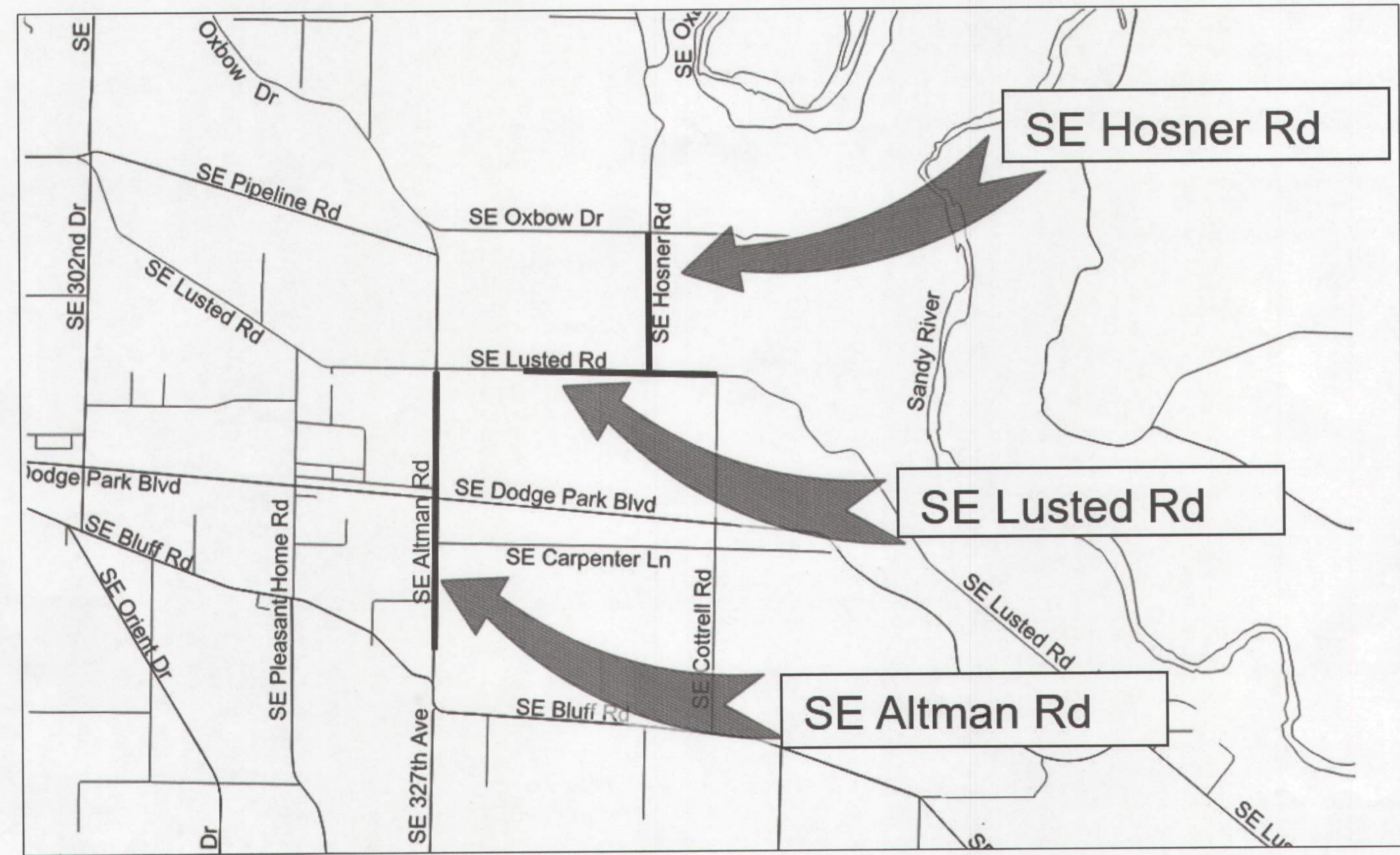
Location Map
NTS

Legend

	Existing	Proposed
Entrance Apron Limits	----	----
Proposed Drain	----	----
Ditch	----	----
Culvert	----	----
Right of Way Line (Property Line)	----	----
Overhead Lines	----	----
Streambed Centerline	----	----
Edge of Gravel	----	----
Edge of Pavement (Driveways)	----	----
Left Edge of Pavement	----	----
Control Line	----	----
Right Edge of Pavement	----	----
Approximate top of cut	----	----
Grade Break	----	----
Tree Line	----	----
Survey Control Point	△	△
Fence	----	----
Utility Pole	●	●
Temp Sed Fence	----	----
Bio Bags	□	□
Catch Basin Inlet	□	□
Sawcut	— SAW —	— SAW —



Project Map
NTS

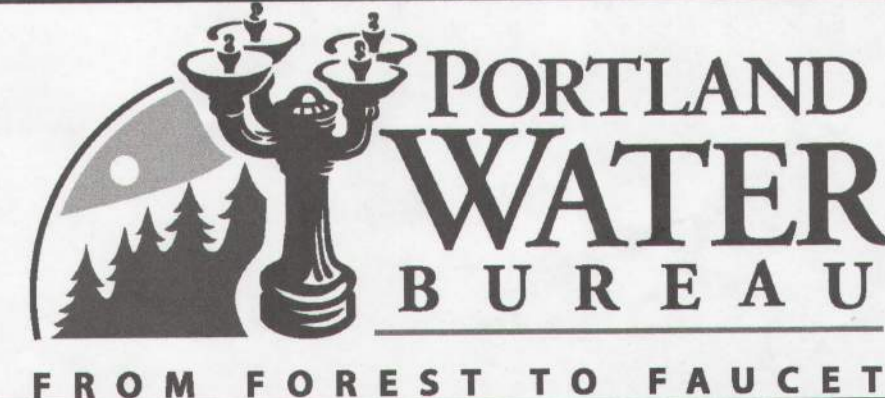
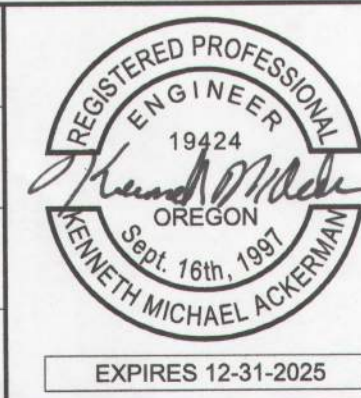


Vicinity Map
NTS

J:\Engineering\CAD\Projects\W022200 - W02299\W02229 - Altman-Hosner-FDR\W02229.dgn 6:26:28 AM 6/26/2024

No	Date	Description	Appd
Revision			

Designed By BJK	Program Mgr DMC
CAD Checked By GWE	Const Mgr BJN
Checked By JDB	Const Supr JDT
Project Mgr KMA	Date



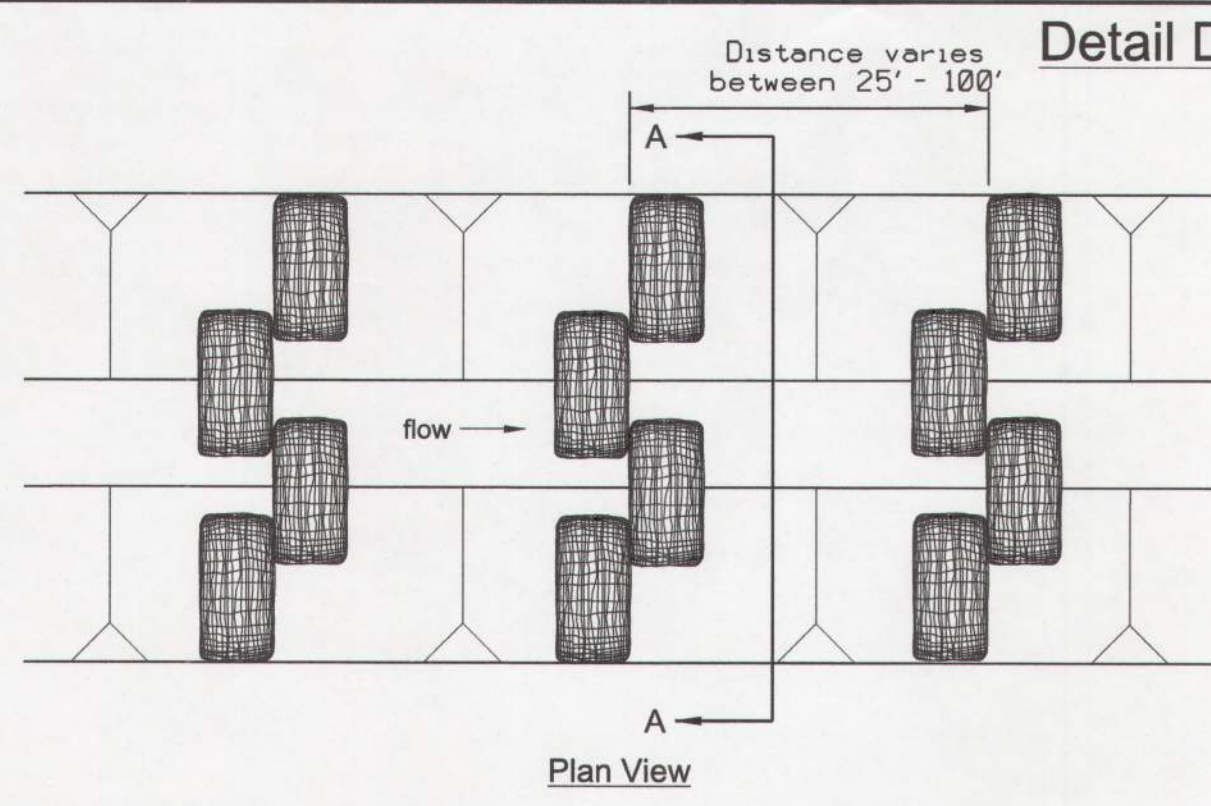
6/27/24
 Jodie A. Inman, Chief Engineer, PE No 61197
 6/27/24
 Kenneth M. Ackerman, Principal Engineer, PE No 19424



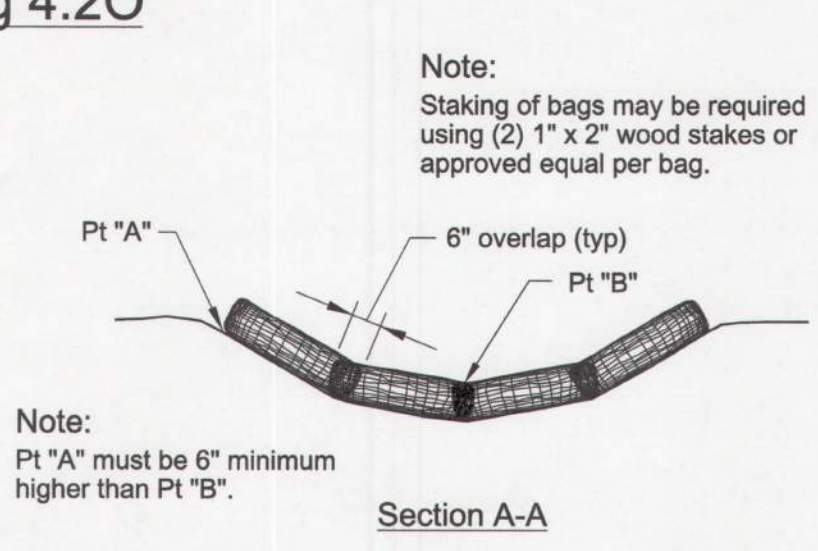
Pavement Rehabilitation Project
SE Altman Rd - SE Lusted Rd
SE Hosner Rd
 Title Sheet

Township/Range 1S4E15, 1S4E16 1S4E21, 1S4E22
Street Opening Permit Number and Date
Synergen Work Order Number
Locate Call In Reference Number

SAP Project No W02229
1/4 Section 3863, 3763, 3754, 3765, 3663, 3664, 3665, 3564, 3565
Sheet No 1 of 24

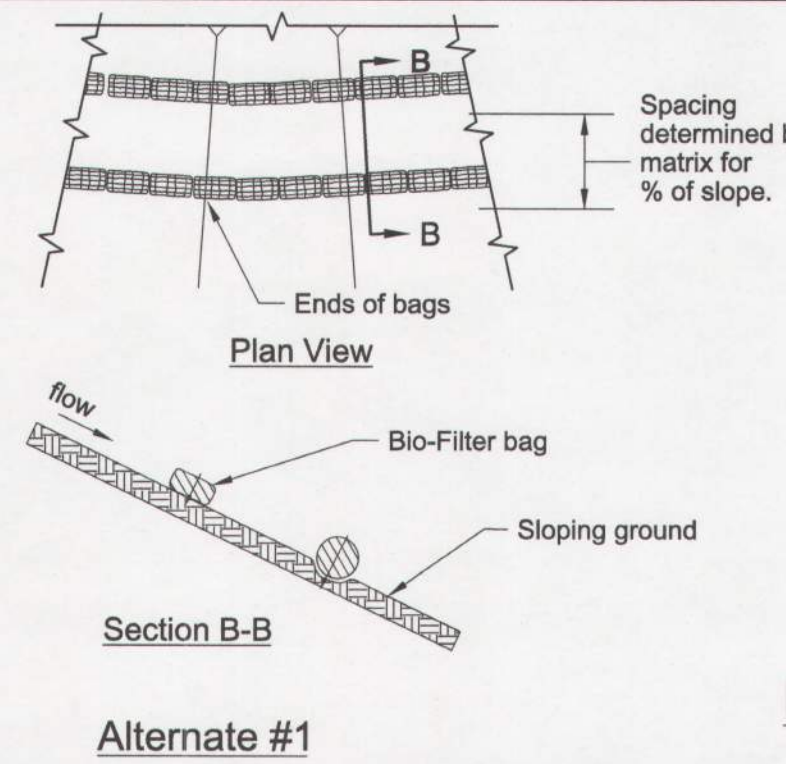


Bio-Filter Bags for Ditches and Swales



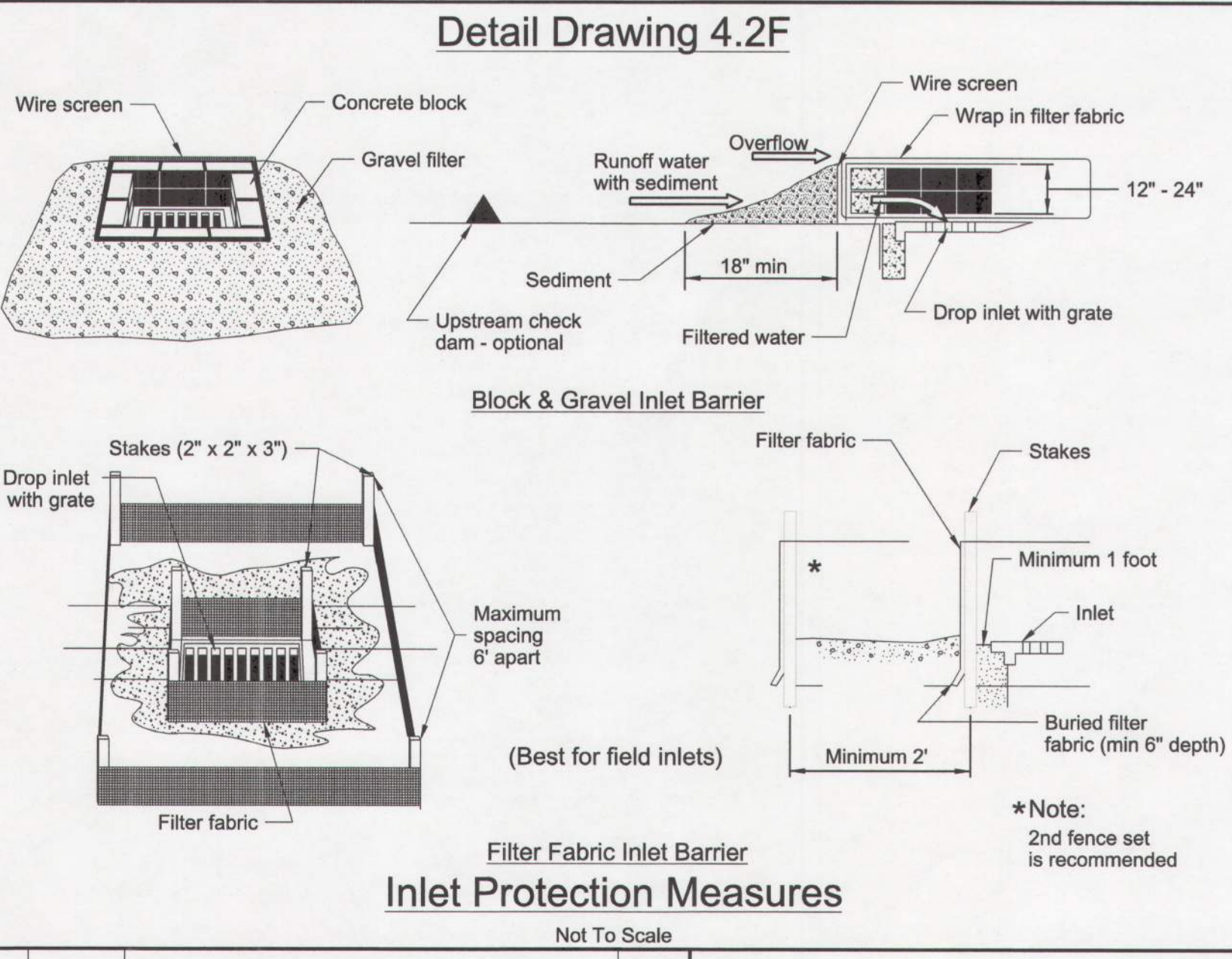
Detail Drawing 4.2B

Slope Installations: Bio-Filter Bags & Fiber Rolls

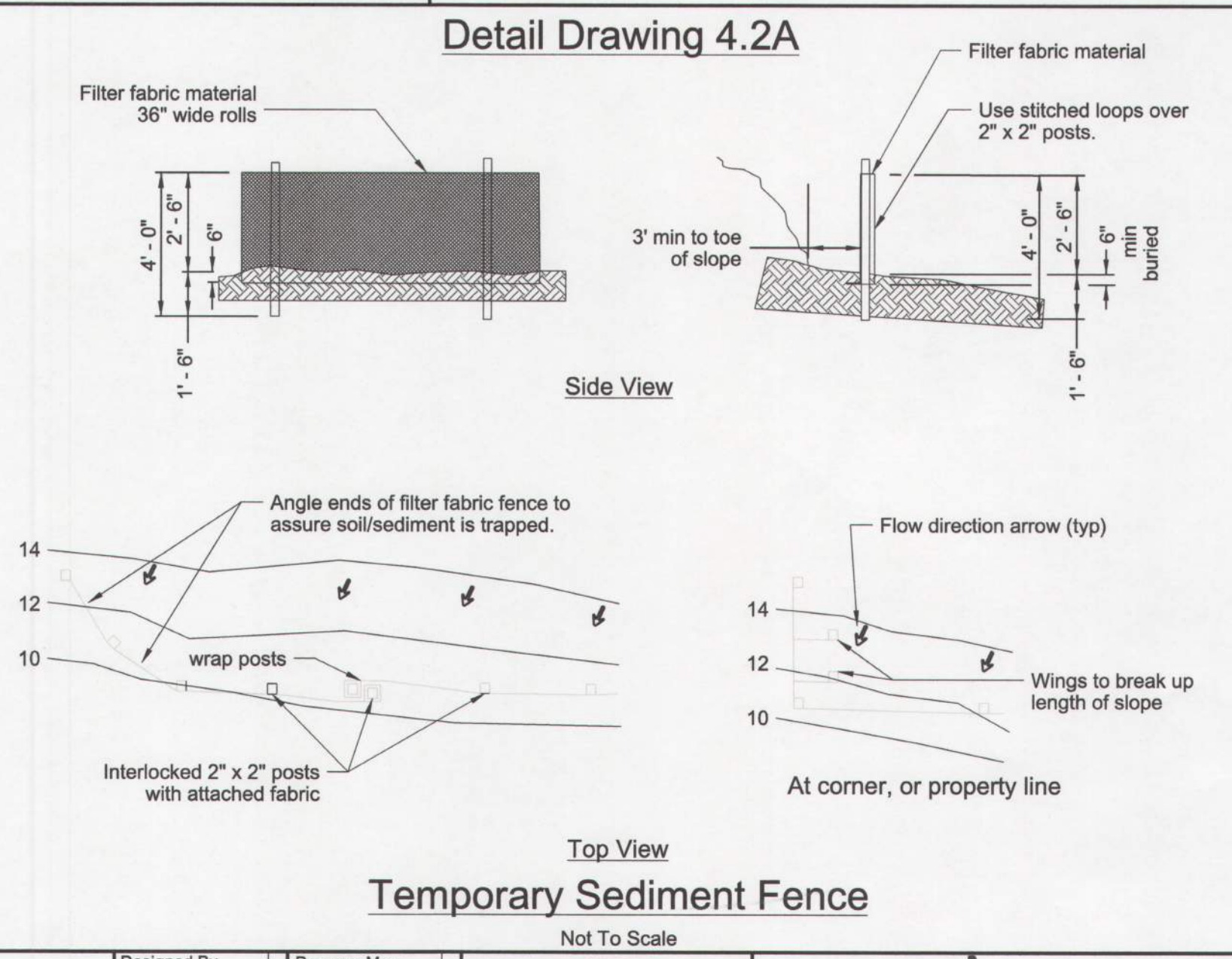


Detail Drawing 4.2H

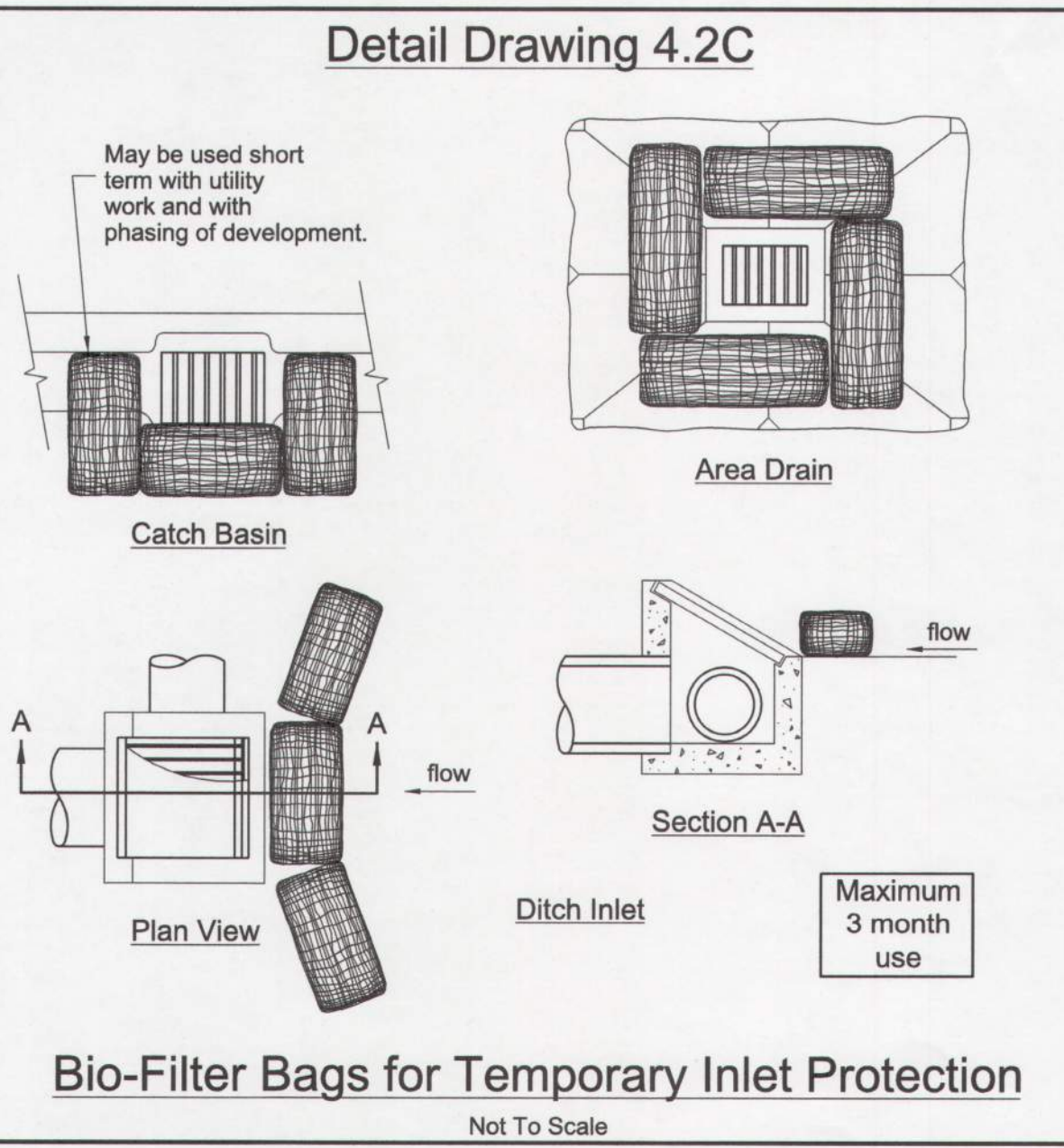
Filter Fabric Inlet Protection



Inlet Protection Measures



Temporary Sediment Fence



Bio-Filter Bags for Temporary Inlet Protection

County Standard Erosion And Sedimentation Control Notes:

STANDARD EROSION AND SEDIMENTATION CONTROL:

UNDER NO CONDITION SHALL SEDIMENT BE WASHED INTO THE STORM SEWER SYSTEM OR DRAINAGE WAYS.

STORM DRAIN INLETS, CATCHBASINS, AND AREA DRAINS SHALL BE PROTECTED USING THE APPROVED BEST MANAGEMENT PRACTICE (BMP) AT ALL TIMES DURING CONSTRUCTION.

EFFECTIVE EROSION CONTROL, DUST CONTROL, AND DRAINAGE CONTROL IS REQUIRED AT ALL TIMES. THE COUNTY MAY ORDER STOPPAGE OF WORK TO EFFECT CORRECTIVE ACTION AT ANY TIME.

APPLY TEMPORARY AND PERMANENT SOIL STABILIZATION MEASURES ON ALL DISTURBED AREAS AS GRADING PROGRESSES.

CONSTRUCTION ACTIVITIES MUST AVOID OR MINIMIZE EXCAVATION AND CREATION OF BARE GROUND FROM OCTOBER 1 THROUGH MAY 31.

DURING WET WEATHER PERIODS TEMPORARY STABILIZATION OF THE SITE MUST OCCUR AT THE END OF EACH WORK DAY IF RAINFALL IS FORECAST IN THE NEXT 24 HOURS.

ALL EROSION AND SEDIMENT CONTROLS NOT IN THE DIRECT PATH OF WORK MUST BE INSTALLED PRIOR TO ANY LAND DISTURBANCE.

PRESERVE EXISTING VEGETATION AND RE-VEGETATE OPEN AREAS WHEN PRACTICABLE BEFORE AND AFTER GRADING OR CONSTRUCTION.

ALL TEMPORARY SEDIMENT CONTROLS MUST REMAIN IN PLACE UNTIL PERMANENT VEGETATION OR OTHER PERMANENT COVERING OF EXPOSED SOIL IS ESTABLISHED.

SEDIMENT CONTROLS MUST BE INSTALLED AND MAINTAINED ON ALL DOWN GRADIENT SIDES OF THE CONSTRUCTION SITE AT ALL TIMES DURING CONSTRUCTION.

WATERTIGHT TRUCKS MUST BE USED TO TRANSPORT SATURATED SOILS FROM THE CONSTRUCTION SITE. AN APPROVED EQUIVALENT IS TO DRAIN THE SOIL ON-SITE AT A DESIGNATED LOCATION USING APPROPRIATE BMP'S; SOIL MUST BE DRAINED SUFFICIENTLY FOR MINIMAL SPILLAGE.

TEMPORARY STABILIZATION OR COVERING OF SOIL STOCKPILES MUST OCCUR AT THE END OF EACH WORK DAY OR OTHER BMP'S MUST BE IMPLEMENTED TO PREVENT TURBID DISCHARGES TO SURFACE WATERS.

DEVELOP AND MAINTAIN ONSITE A WRITTEN SPILL PREVENTION AND RESPONSE PROCEDURE.

ANY USE OF TOXIC OR OTHER HAZARDOUS MATERIALS MUST INCLUDE PROPER STORAGE, APPLICATION, AND DISPOSAL.

THE PERMITEE MUST PROPERLY PREVENT AND MANAGE HAZARDOUS WASTE, USED OILS, CONTAMINATED SOILS, CONCRETE WASTE, SANITARY WASTE, LIQUID WASTE, OR OTHER TOXIC SUBSTANCES DISCOVERED OR GENERATED DURING CONSTRUCTION.

SIGNIFICANT AMOUNTS OF SEDIMENT WHICH LEAVE THE SITE MUST BE CLEANED UP WITHIN 24 HOURS AND PLACED BACK ON THE SITE AND STABILIZED OR PROPERLY DISPOSED. THE CAUSE OF THE SEDIMENT RELEASE MUST BE FOUND AND PREVENTED FROM CAUSING A REOCCURRENCE OF THE DISCHARGED WITHIN THE SAME 24 HOURS. ANY IN-STREAM CLEAN UP OF SEDIMENT SHALL BE PERFORMED ACCORDING TO THE OREGON DIVISION OF STATE LANDS REQUIRED TIME FRAME.

SEDIMENT MUST NOT BE INTENTIONALLY WASHED INTO STORM SEWERS, DRAINAGE WAYS, OR WATER BODIES. DRY SWEEPING MUST BE USED TO CLEAN UP RELEASED SEDIMENTS.

THE APPLICATION RATE OF FERTILIZERS USED TO REESTABLISH VEGETATION MUST FOLLOW THE MANUFACTURER'S RECOMMENDATIONS. NUTRIENT RELEASES FROM FERTILIZERS TO SURFACE WATERS MUST BE MINIMIZED. TIME RELEASE FERTILIZERS SHOULD BE USED AND CARE SHOULD BE TAKEN IN THE APPLICATION OF FERTILIZERS WITHIN ANY WATER WAY RIPARIAN ZONE.

Legend

- Catch Basin Inlet
- Edge of Roadway
- Project Area
- Area of Concern*

* Area to be determined by Portland Water Bureau engineer or Project Manager.

Note: For additional information on detail drawings see the most recent version of the Bureau of Development Services Erosion Control Manual.

Erosion Control Notes

- General**
- Erosion Control is required for this project. Erosion Control measures (or ESC facilities) must be installed and maintained in accordance with the most recent version of the City of Portland's, Bureau of Development Service Erosion Control Manual, the approved Erosion, Sediment and Pollution Control Plan (ESPCP) and the Special Provisions. Selected Standard drawings from the Erosion Control Manual are reproduced on this sheet. However, a current Erosion Control Manual must be kept on site for installation and maintenance specifications, and for guidance in preparing alternative Erosion Control.
 - Approval of the Erosion, Sediment and Pollution Control Plan (ESPCP) does not constitute an approval or permanent road or drainage design (EG: size and location of roads, pipes, restrictors, channels, retention facilities, utilities, etc).
 - Approval of the Erosion, Sediment and Pollution Control Plan (ESPCP) does not relieve the Operation Supervisor's/Contractor's responsibility to ensure that the approved Erosion Control best management practices are constructed and maintained to contain sediment and pollutants on the construction site. The implementation of the ESPCP and the construction, maintenance, replacement and upgrading of the ESPCP facilities is the responsibility of the Operations Supervisor/Contractor until all construction is completed and approved, and (where necessary) vegetation/landscaping is established.
- At Beginning of Construction**
- The boundaries of the clearing limits shown on the plan shall be clearly flagged in the field prior to construction. During the construction period, no disturbance beyond the flagged clearing limits shall be permitted. The flagging shall be maintained by the Operations Supervisor/Contractor for the duration of construction.
 - Stabilized construction entrances shall be installed (where necessary) at the beginning of construction and maintained for the duration of the project. Additional measures, such as wheel washes or hand brushing, may also be necessary to insure that all paved areas are kept clean for the duration of the project.
 - The ESPCP facilities shown on the plan must be constructed in conjunction with all clearing and grading activities, and in such a manner as to insure that sediment and sediment-laden water do not enter the drainage system, roadways or violate applicable water standards.
- During Construction**
- The ESPCP facilities shown on the plan are the minimum requirements for anticipated site conditions. During the construction period, the ESPCP facilities shall be upgraded as needed for the unexpected storm events and to ensure that sediment and sediment-laden water do not leave the site.
 - The ESPCP facilities shall be inspected daily by the Operations Supervisor/Contractor and maintained as necessary to ensure their continued functioning.
 - The ESPCP facilities on inactive sites shall be inspected and maintained a minimum of once a month or within the 24 hours following a storm event.
 - Gravel, matting or plating shall be used to protect the ground surface adjacent to water line construction within the project. A durable and temporary access way shall be selected and designed to be effective in preventing erosion, wheel rutting and excessive compaction. The access way, including all imported material, must be removed after construction.
 - Storm water inlets that are functioning during the course of construction shall be protected by approved sediment control measures so that sediment-laden water can not enter the inlets without first being filtered.
 - At no time shall more than one (1) foot of sediment be allowed to accumulate within a trapped catch basin. All catch basins and conveyance lines shall be cleaned prior to paving. The cleaning operation shall not flush sediment-laden water into the downstream system.
 - Any stockpiled soil must be secured and protected throughout the project with soil stabilization measures including sediment barriers and plastic sheeting. Stockpiling within a wetland must be avoided. However, if temporary stockpiling occurs within a wetland, stockpiles must be covered nightly with plastic per the requirements of the attached DSL permit. The Operations Supervisor/Contractor is accountable for the protection of all stockpiles on the site, and those transported from the site. Deposition of soil may be subject to additional regulation requiring permit, review or Erosion and Sediment Control.
 - Any soil tracked onto the street from established construction entrances must be swept up and disposed of on site. Streets must not be washed to remove tracked soil.
 - An Erosion Control Inspection Log must be kept on site for review by city inspection staff. The log shall conform to the project special specifications.
- After Construction**
- Erosion control measures shall continue to be monitored, improved and maintained on completed stages of work, until all project work areas are stable.
 - A vegetation plan may need to be implemented when necessary. Any replacement vegetative cover planted by Operations Supervisor/Contractor must not include plants listed in either the nuisance or the prohibited plant list, as set forth in the city of Portland plant list.
- Sediment Fence Notes**
- The filter fabric shall be purchased in a continuous roll cut to length of the barrier to avoid use of joints. When joints are necessary, filter cloth shall be spliced together only at a support post, with a minimum 6-inch overlap, and both ends securely fastened to the post, or overlap 2-inch x 2-inch posts and attach as shown on detail 4.2A.
 - The filter fabric fence shall be installed to follow the contours where feasible. The fence posts shall be spaced a maximum of 6 feet apart and driven securely into the ground a minimum of 24 inches.
 - The filter fabric shall have a minimum vertical burial of 6 inches. All excavated material from filter fabric fence installation, shall be back filled and compacted, along the entire disturbed area.
 - Standard or heavy-duty filter fabric fence shall have manufactured stitched loops for 2-inch x 2-inch post installation. Stitched loops shall be installed on the uphill side of the slope.
 - Filter fabric fences shall be removed when they have served their useful purpose, but not before the up slope area has been permanently protected and stabilized.
 - Filter fabric fences shall be inspected by the Operations Supervisor/Contractor immediately after each rainfall and at least daily during prolonged rainfall. Any required repairs shall be made immediately.
- Note:** Location of inlets are based on aerial mapping. Contractor will be responsible for field verifying locations of all inlets and providing erosion control.

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Survey	Unknown	Unknown	

Designed By	BJK	Program Mgr	DMC
CAD Checked By	GWE	Const Mgr	BJN
Checked By	JDB	Const Supr	JDT
Project Mgr	KMA	Date	

Kenneth M. Ackerman, Principal Engineer, PE No 19424

Date: 8/19/24

Pavement Rehabilitation Project
SE Altman Rd - SE Lusted Rd
SE Hosner Rd
 Erosion Control Standard Details Sheet

SAP Project No
W02229
 1/4 Section
 3883, 3763, 3784,
 3765, 3663, 3664,
 3665, 3564, 3565
 Sheet No
3 of 24

Multnomah County Land Use Decision T3-2022-16220 Conditions of Approval:

1. Additions to County Transportation Condition 7

The TCP must include an emergency coordination section that at minimum includes the following:

iv. Satisfy the minimum requirements of the Manual on Uniform Traffic Control Devices for Streets and Highways;

v. Provide construction update reports to emergency responders that include, at a minimum, the following information:

O. Dates and times of closure/partial closure

P. Name of contractor and emergency contacts (required on-site contact)

Q. Purpose of closure

R. Location of closure and number of lanes

S. Work hours and times of road closures

T. Traffic control layout plan

U. Legend

•North arrow

•Street names within a certain distance of the site

•Physical features such as medians, shoulders, etc.

•Identified method for passage of emergency vehicles (including temporary conditions/detour plan)

•Location of significant construction items such as dumpsters and heavy equipment

vi. The construction update reports must be provided at least weekly unless an alternative frequency is requested by an emergency responder.

The TCP must provide for access through construction zones as follows:

vii. Where no detour is available, such as to access Lusted Flats via Dodge Park Boulevard or to access the only access to a farm field, the applicant shall (1) ensure that work zones allowing a single lane of traffic to pass are wide enough to accommodate farm traffic up to 16 feet wide; and (2) flag farm traffic, service providers, and local residents (within the closure) through otherwise closed work zones.

viii. The Water Bureau shall (1) ensure that work zones allowing a single lane of traffic to pass are wide enough to accommodate emergency vehicles; and (2) flag emergency vehicles through otherwise closed work zones. Access for emergency vehicles shall be provided at all times.

ix. The Water Bureau shall require the contractor to take measures to ensure they can accommodate this traffic through a work zone regardless of the stage of construction. For example, if pipeline construction obstructs a road that cannot be detoured around, the contractor will have on-hand the materials needed to plate the excavation or otherwise allow this traffic to proceed through the work zone.

E.1.h. When construction impacts the public right-of-way in front of a business, post "business open" signs typical of roadway construction projects in any area where road construction and/or pipeline installation is occurring and where business would anticipate visitors.

E.1.m [Carpenter West of Cottrell] Provide "local access only" signage restricting access to Carpenter Lane west of Cottrell Road, as well as including the prohibition on use in the safe driver training.

E.1.r Instruct filtration project construction drivers to avoid specific road segments that have direct access to identified schools. The specific school, streets, types of construction traffic, and hours to be avoided are listed in the table below. These constraints apply only on days when school is in session.

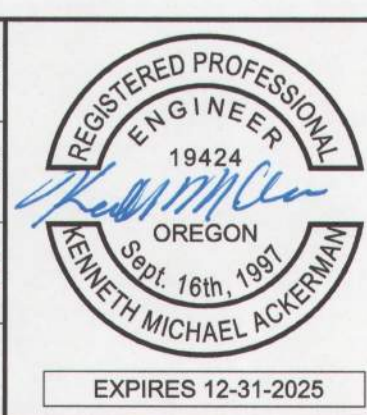
District	School	Street	Extent	Construction Traffic Type	Avoidance Hours*
Oregon Trail	Oregon Trail Academy	SE Proctor Rd	SE Bluff Rd to SE Dodge Park Blvd	All	All
		SE Bluff Rd	Just east of SE 352 nd Ave to SE Bear Creek Ln	Trucks and craft labor commuters	7:15 to 8:15 am 2:15 to 3:15 pm
	Kelso Elementary	SE Kelso Rd	SE Orient Dr to SE Eklund Ave	All	All
Gresham-Barlow	Sam Barlow High	SE Lusted Rd	SE 282 nd Ave to SE 302 nd Ave	Trucks Craft labor commuters	All 7:05 to 9:05 am 2:00 to 3:00 pm
		SE 302 nd Ave	SE Lusted Rd to SE Chase Rd	Trucks Craft labor commuters	All 7:05 to 9:05 am 2:00 to 3:00 pm
	East Orient Elementary	SE 302 nd Ave	SE Dodge Park Blvd to SE Bluff Rd	All	All
	West Orient Middle	SE Short Rd	SE Dodge Park Blvd to SE Orient Dr	All	All
		SE Orient Dr/SE Bluff Rd	SE Short Rd to SE 302 nd Ave	Trucks Craft labor commuters	All 8:35 to 10:35 am 3:05 to 4:05 pm
	Kelly Creek Elementary	SE Baker Way/SE 24 th St	SE Williams Dr to SE Chase Rd	All	All

E.2 (pg. 84)The Water Bureau may not include Carpenter Lane west of Cottrell as a detour option in traffic control plans for signage during construction.

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Checked By JDB	Const Supr JDT
Project Mgr KMA	Date



Kenneth M. Ackerman
Kenneth M. Ackerman, Principal Engineer, PE No 19424

8/19/24
Date



Pavement Rehabilitation Project
Land Use Conditions of Approval
Multnomah County
Casefile T3-2022-16220
Traffic Control Plan Sheet

SAP Project No
W02229

1/4 Section
3863, 3763, 3764,
3765, 3663, 3664,
3665, 3564, 3565
Sheet No

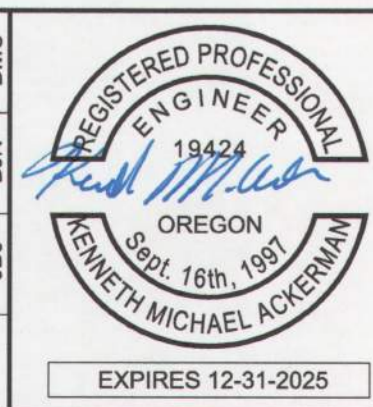
4 of 24

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Checked By JDB	Const Supvr JDT
Project Mgr KMA	Date



Kenneth M. Ackerman
Kenneth M. Ackerman, Principal Engineer, PE No 19424

8/18/24
Date



Pavement Rehabilitation Project
SE Altman Rd
South of SE Dodge Park Blvd

Traffic Control Plan Sheet



SAP Project No
W02229
1/4 Section
3863 & 3763
Sheet No
5 of 24



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Project Mgr KMA	Date



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Pavement Rehabilitation Project
SE Altman Rd
North of SE Dodge Park Blvd
Traffic Control Plan Sheet

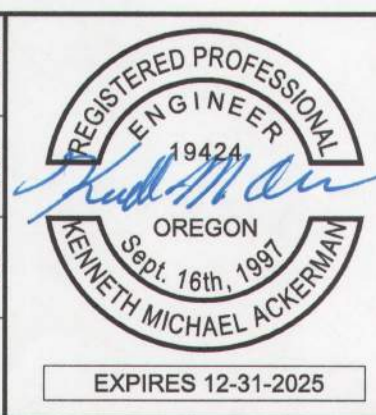
SAP Project No
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 1/4 Section
 3763 & 3663
 Sheet No
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Project Mgr KMA	Date



Kenneth M. Ackerman
Kenneth M. Ackerman, Principal Engineer, PE No 19424
Date: *8/14/24*



Pavement Rehabilitation Project
**SE Lusted Rd
West of SE Cottrell Rd**
Traffic Control Plan Sheet

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Project Mgr KMA	Date



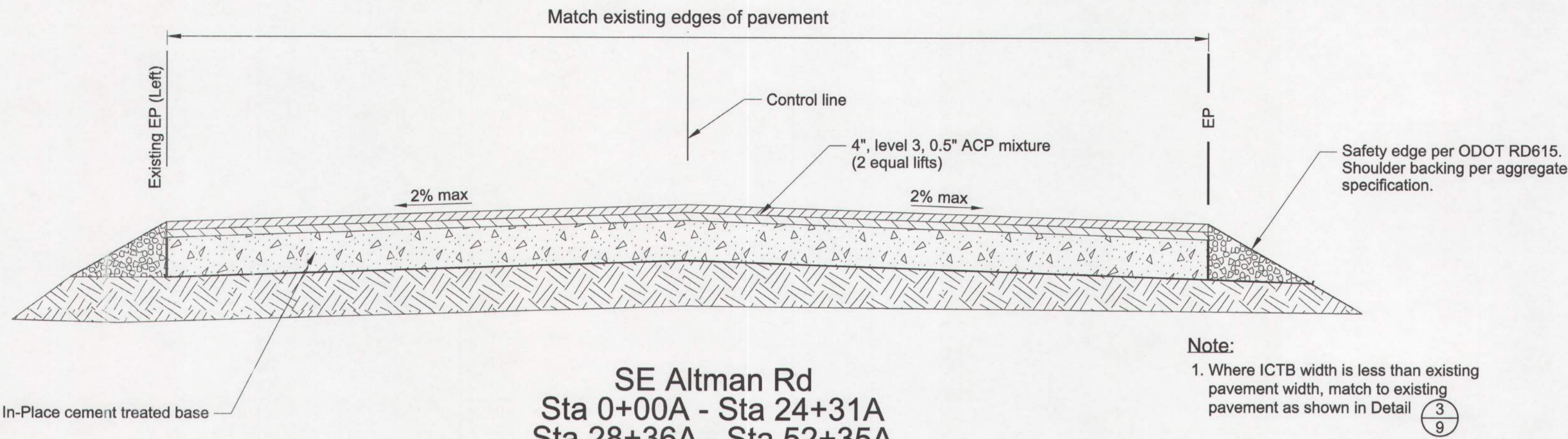
Kenneth M. Ackerman
Kenneth M. Ackerman, Principal Engineer, PE No 19424
8/19/24
Date



Pavement Rehabilitation Project
**SE Hosner Rd
North from SE Lusted Rd**
Traffic Control Plan Sheet

SAP Project No
W02229
1/4 Section
3764, 3765, 3664,
3665, 3564, 3565
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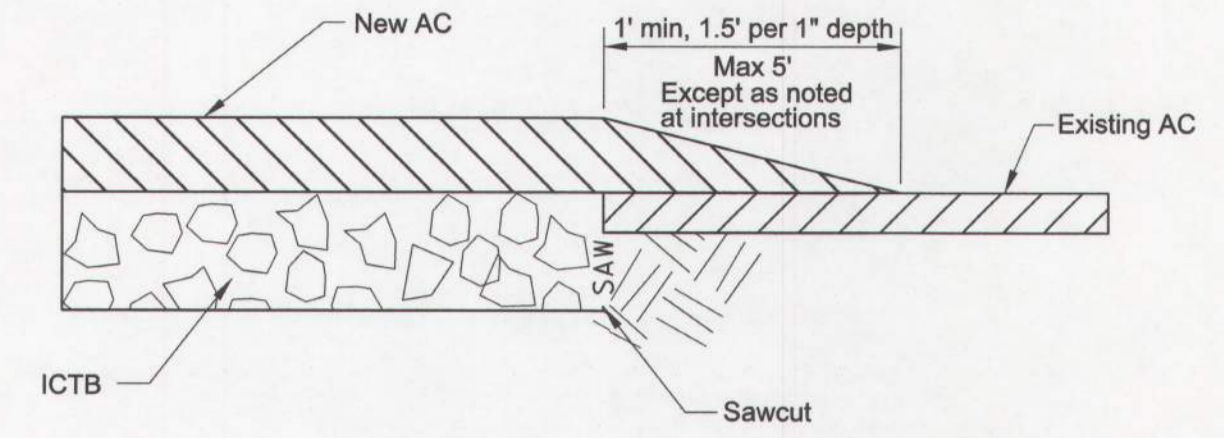
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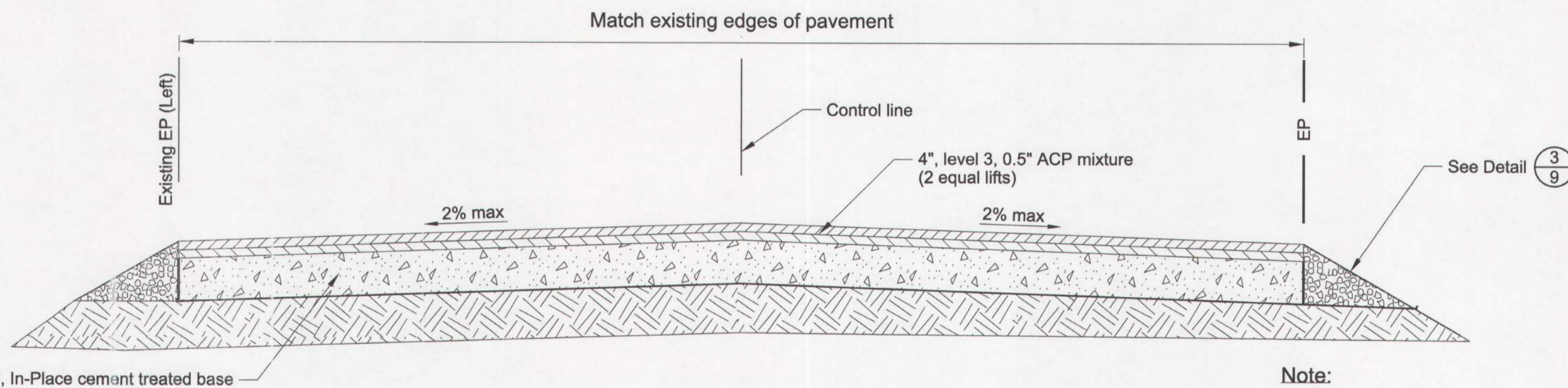
SE Altman Rd
Sta 0+00A - Sta 24+31A
Sta 28+36A - Sta 52+35A
In-Place Cement Treated Base

1
9 NTS

Note:
1. Where ICTB width is less than existing pavement width, match to existing pavement as shown in Detail (3/9)



3
9 NTS ICTB Edge on Existing Asphalt



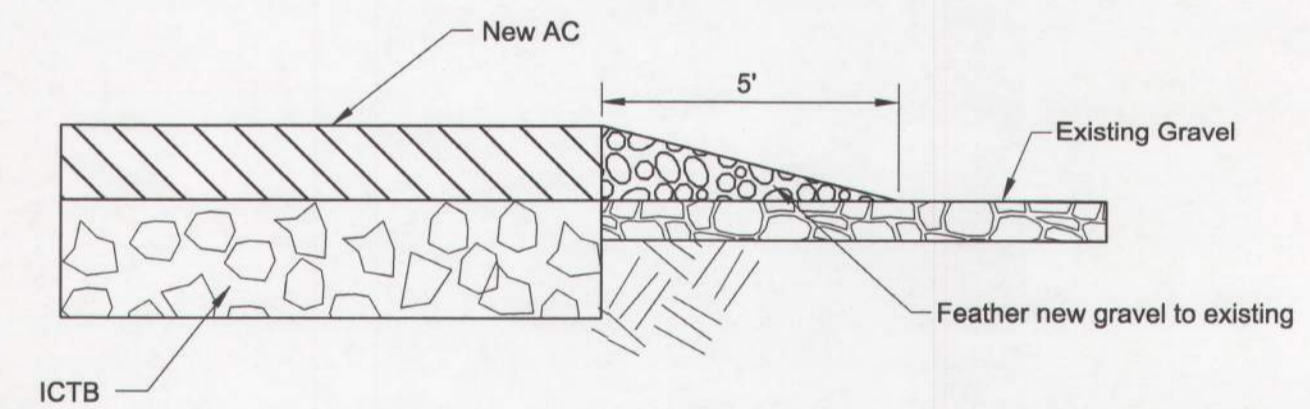
Prior to Paving

1. Full depth recycled: 11.5"
2. Excavate: 3.0"
3. Cement treat remaining: 8.5"
4. Compaction per Specifications.

SE Altman Rd
Sta 24+31A - Sta 28+14A
In-Place Cement Treated Base

2
9 NTS

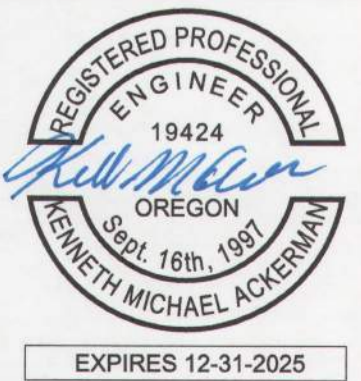
Note:
1. Where ICTB width is less than existing pavement width, match to existing pavement as shown in Detail (4/9)



4
9 NTS ICTB Edge on Existing Gravel Driveways

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Checked By JDB	Const Supvr JDT
Project Mgr KMA	Date

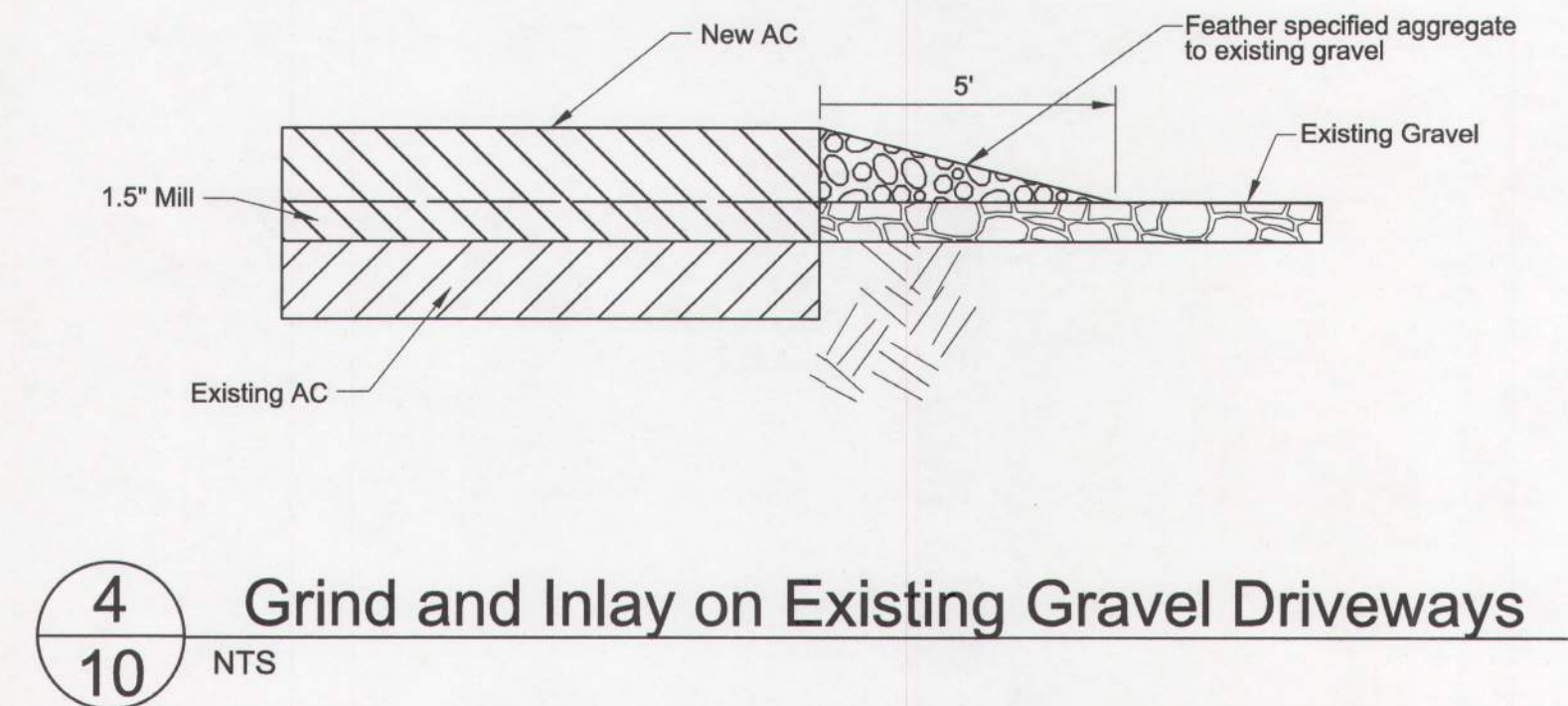
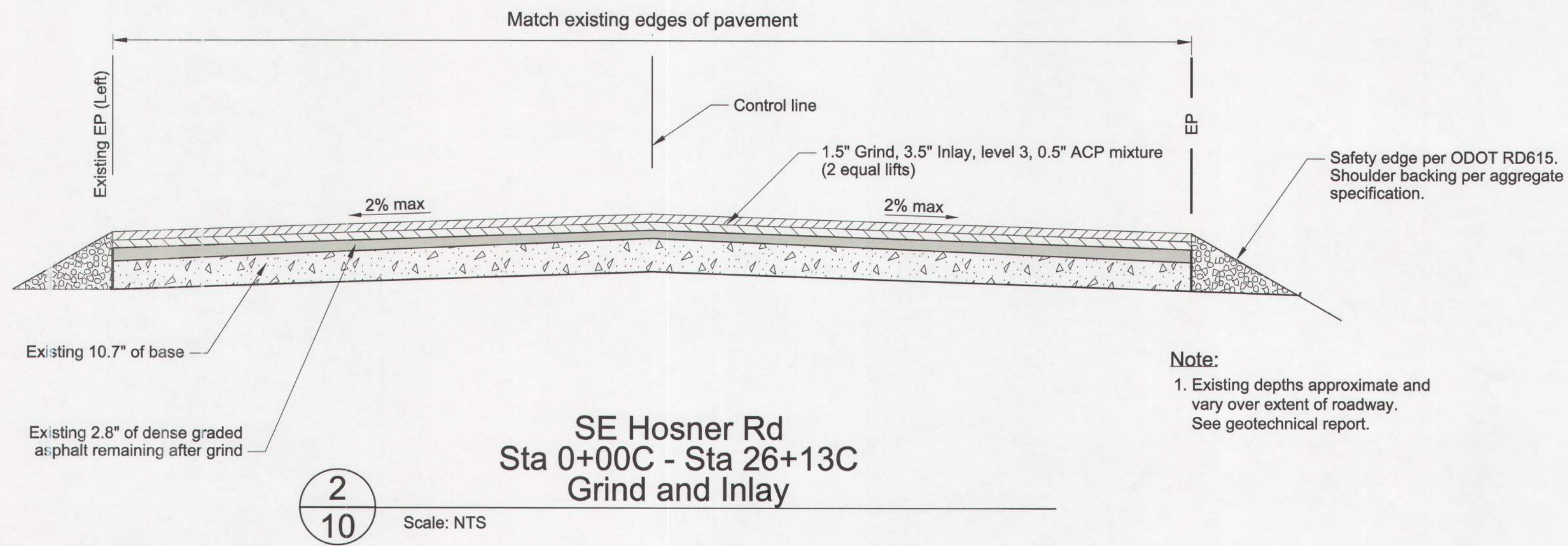
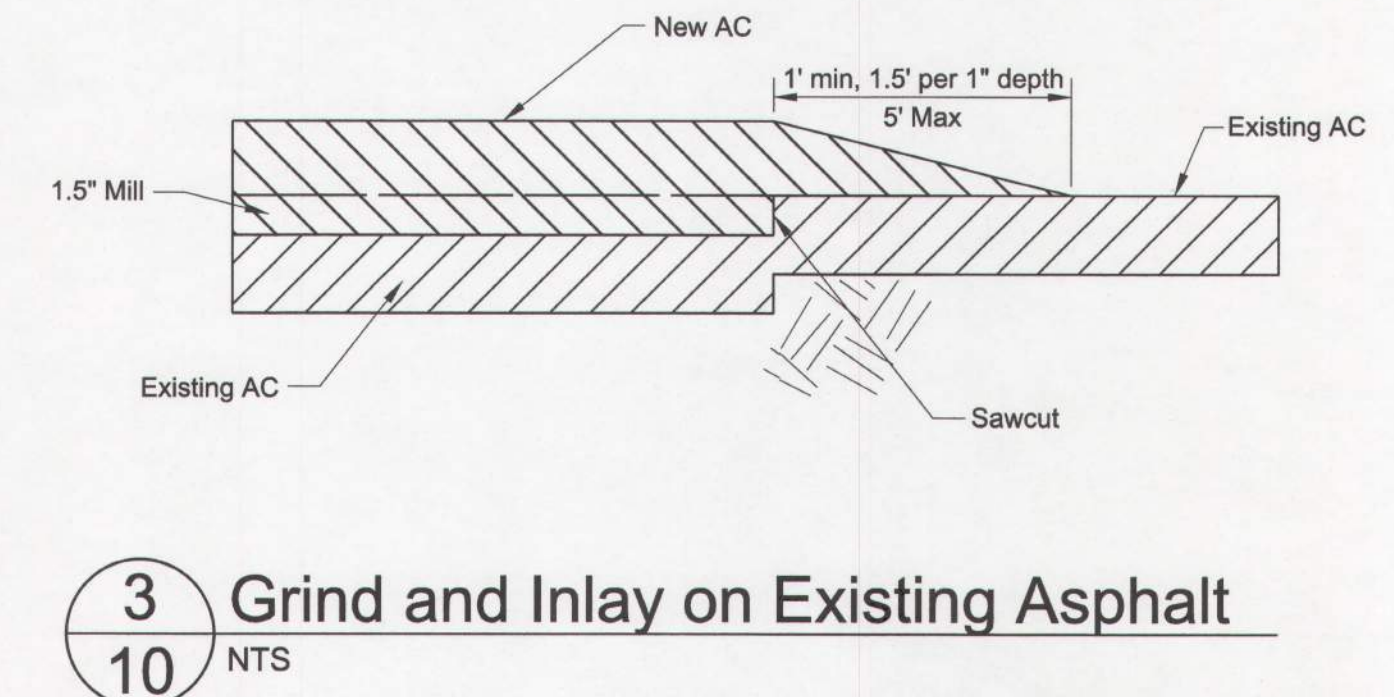
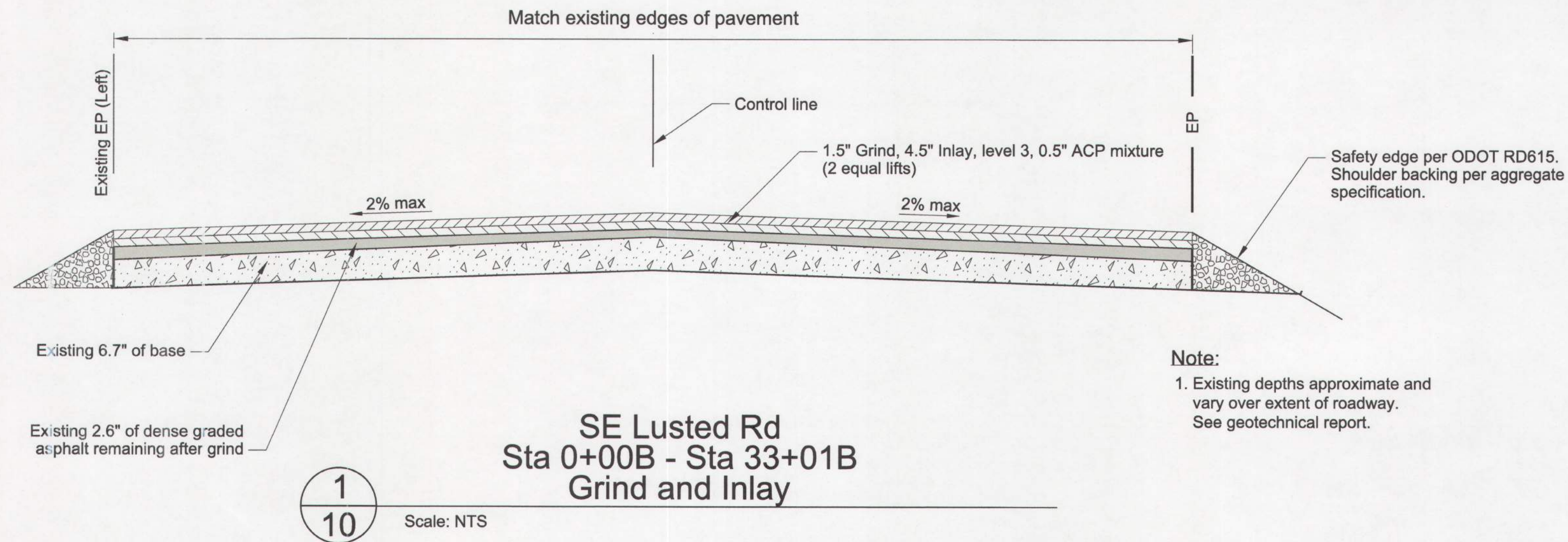


Kenneth M. Ackerman
Kenneth M. Ackerman, Principal Engineer, PE No 19424
Date: 8/18/24



Pavement Rehabilitation Project
SE Altman Rd - SE Lusted Rd
SE Hosner Rd
Paving Details Sheet
SE Altman Rd Sta 0+00A to Sta 52.35A

SAP Project No
W02229
1/4 Section
3663, 3763, 3764,
3765, 3663, 3664,
3665, 3564, 3565
Sheet No
9 of 24



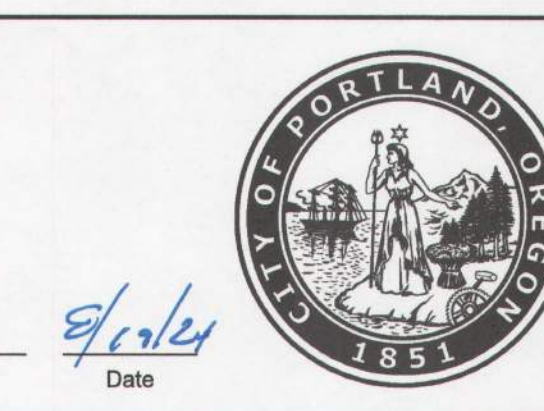
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Project Mgr KMA	Date

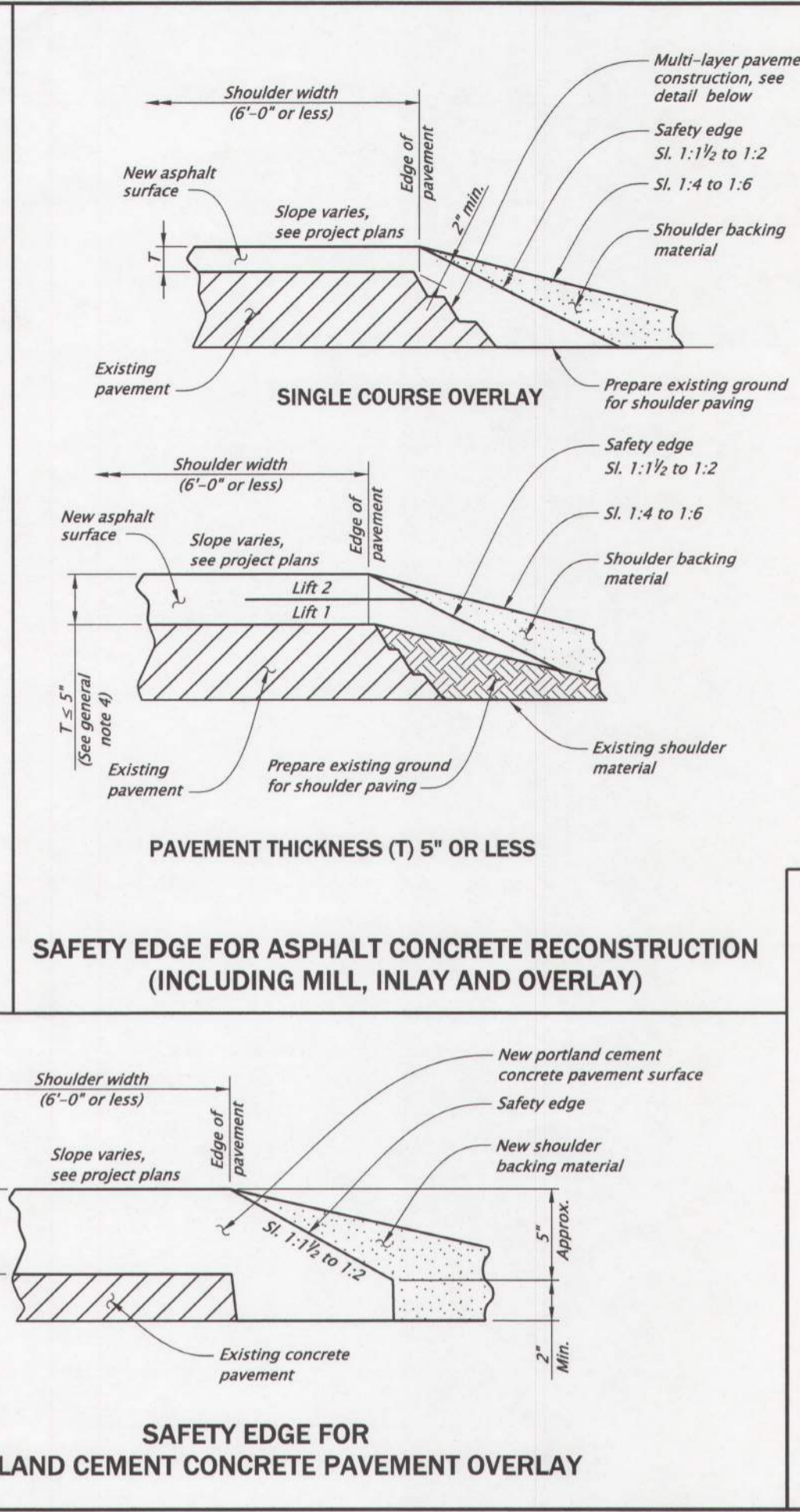
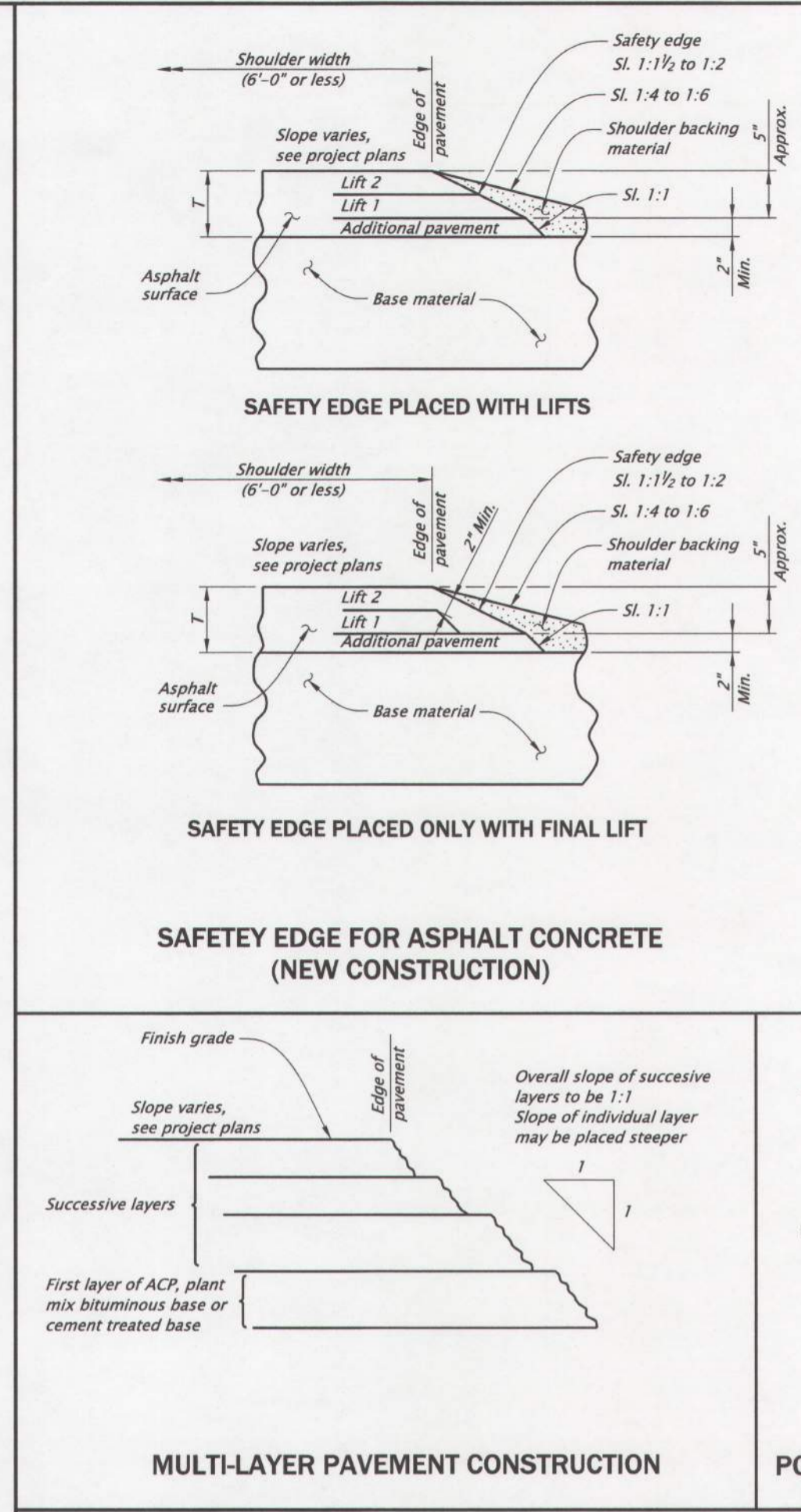
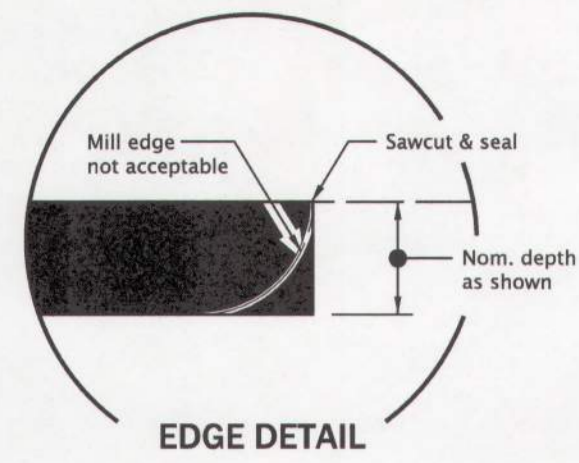
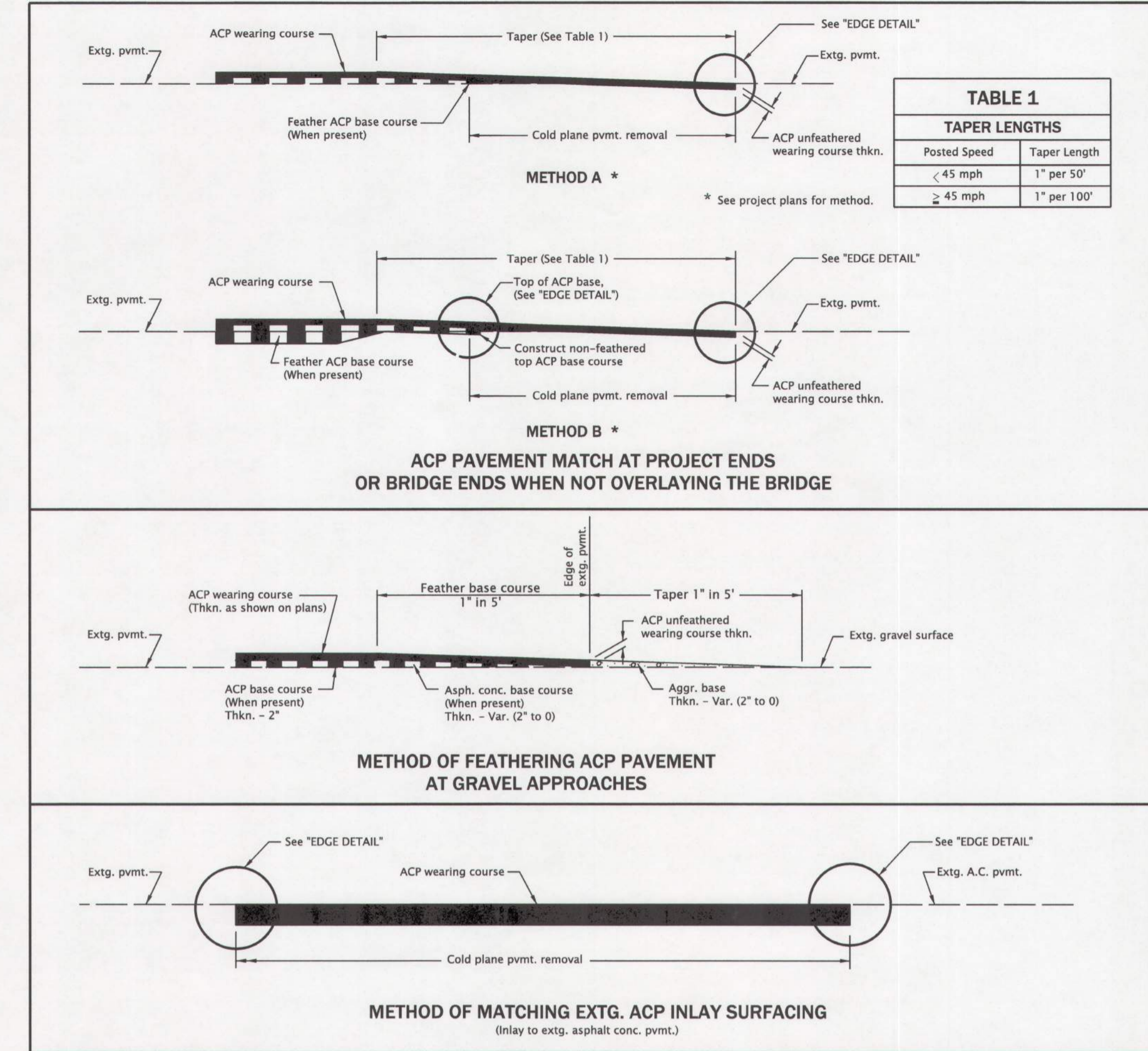


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Pavement Rehabilitation Project
SE Altman Rd - SE Lusted Rd
SE Hosner Rd
Paving Details Sheet
SE Lusted Rd - Sta 0+00B to Sta 33+01B
SE Hosner Rd - Sta 0+00C to Sta 26+13C

SAP Project No
W02229
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3663, 3763, 3764,
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- GENERAL NOTES FOR ALL DETAILS ON THIS SHEET:**
- Safety edges are required at the outside edges of the paved roadway (edge of travel lane or edge of paved shoulders), where the wearing surface thickness is 2" or greater, except where indicated in the plans.
 - Construct the safety edge at a slope of 1:1 1/2 to 1:2 measured from the pavement surface.
 - Do not construct safety edge at intersections, paved drives, or other obstructions.
 - For total new asphalt depth of "T" > 5", construct the safety edge to the full thickness of the surface and intermediate courses. For total new asphalt depth of "T" > 5", construct the safety edge to a depth of 5" approximately with a 1:1 sloped face below the safety edge.

1
11 NTS
ODOT - RD610 Asphalt Concrete Pavement (ACP) Details

2
11 NTS
ODOT - RD615 Surface Edge Details

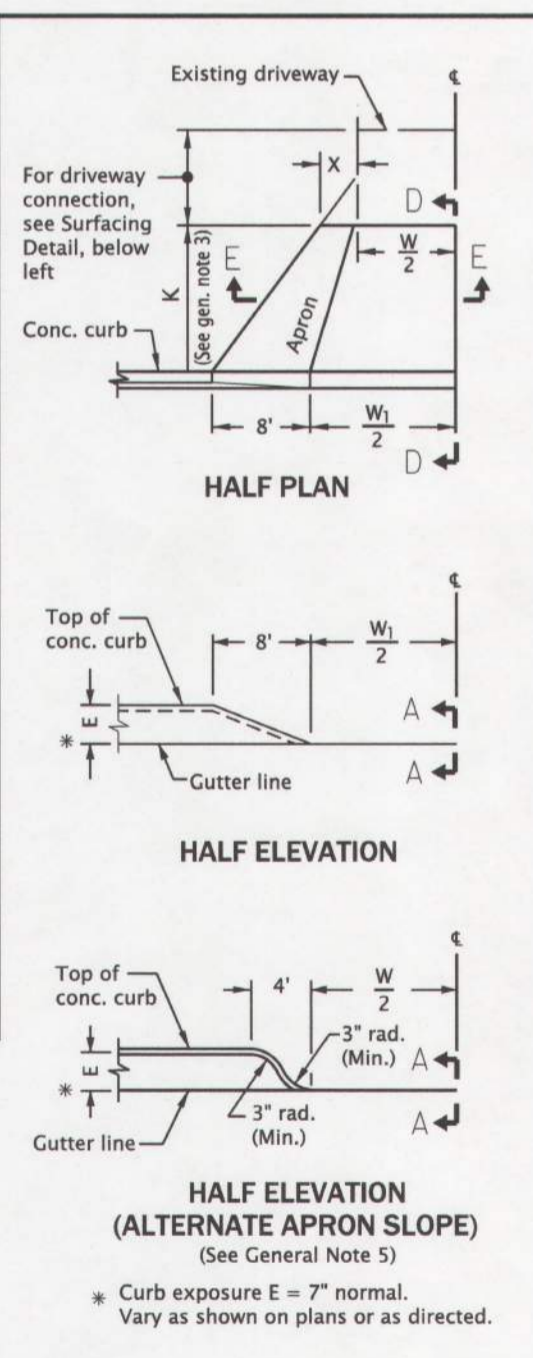
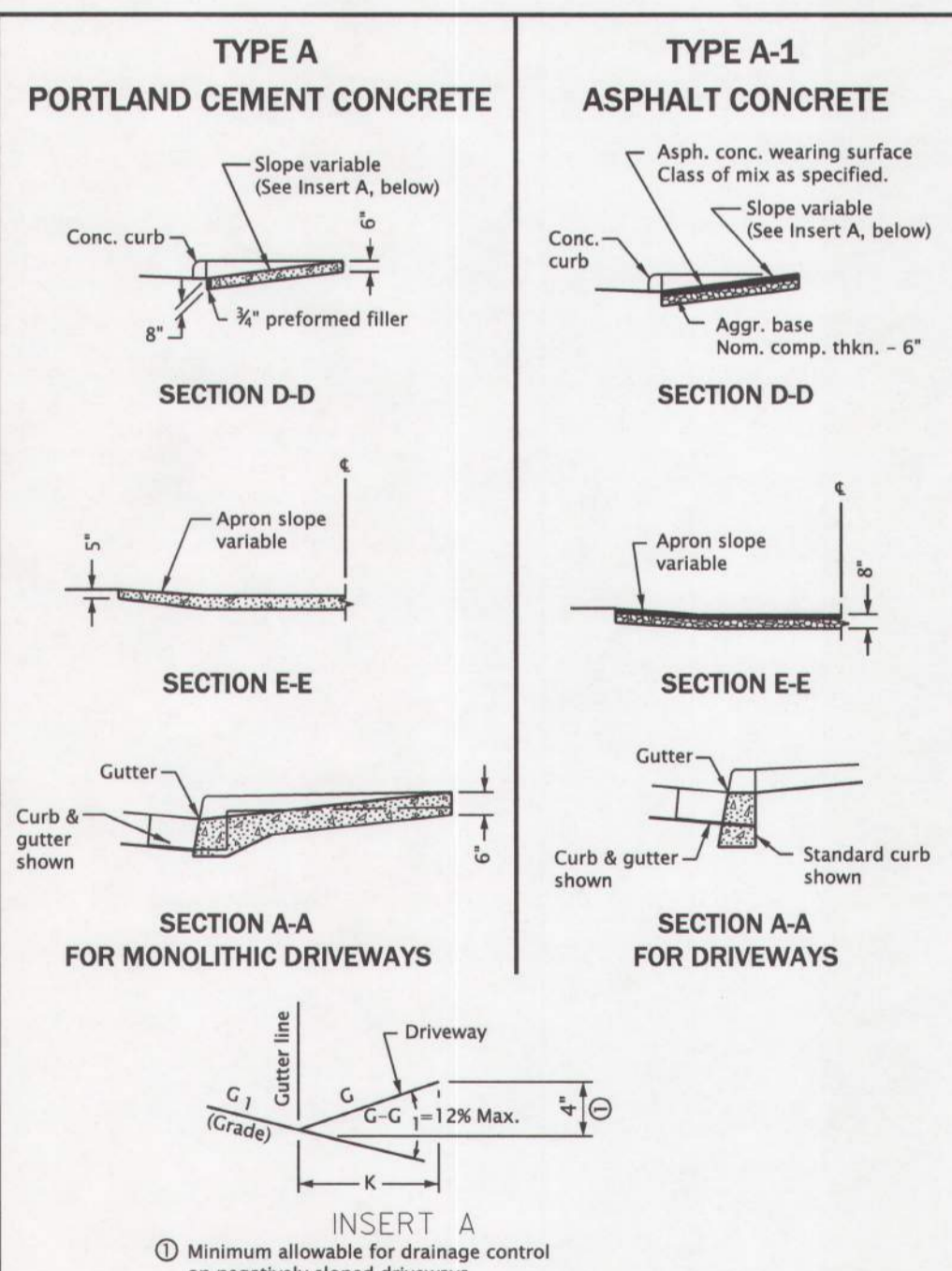
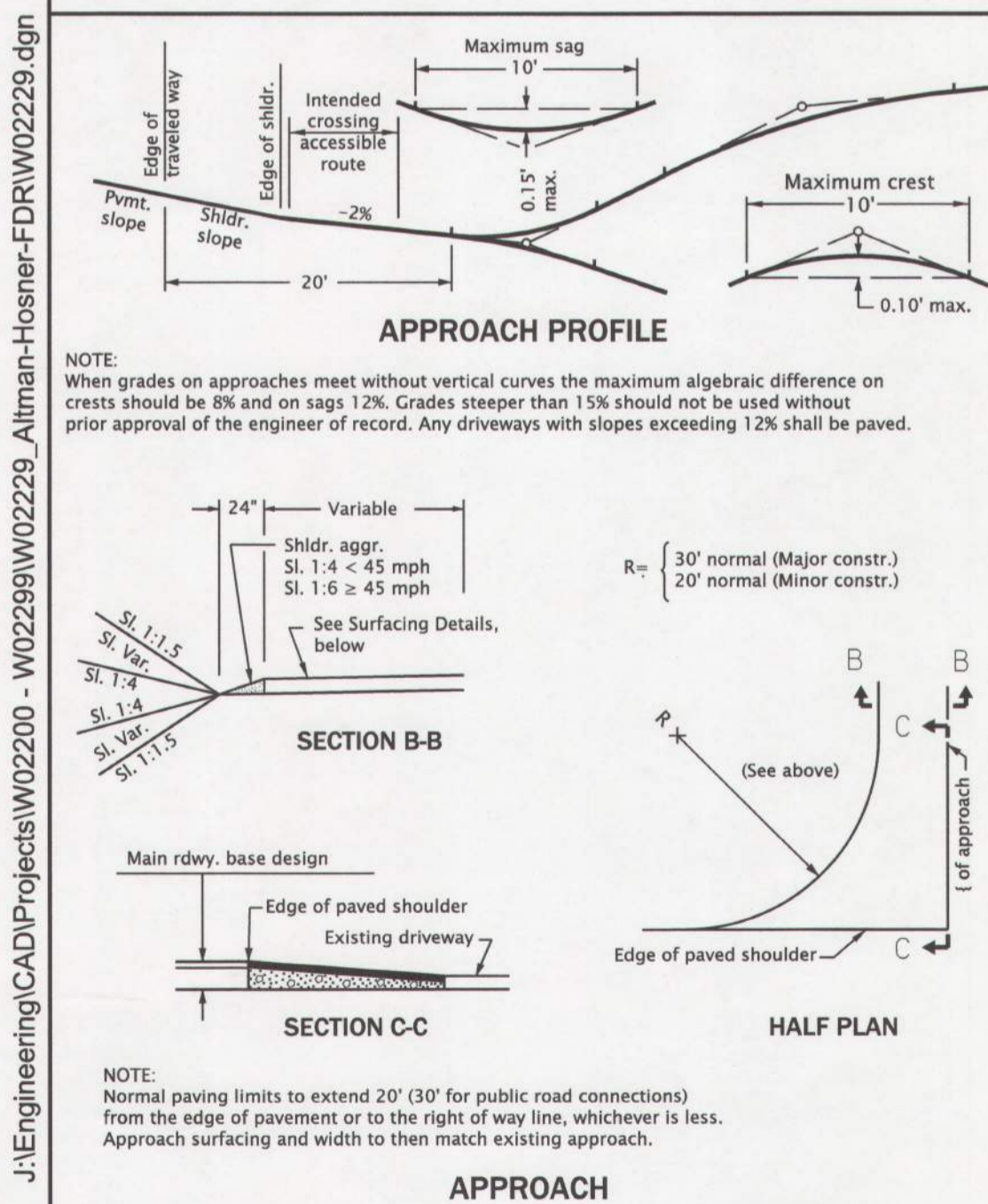
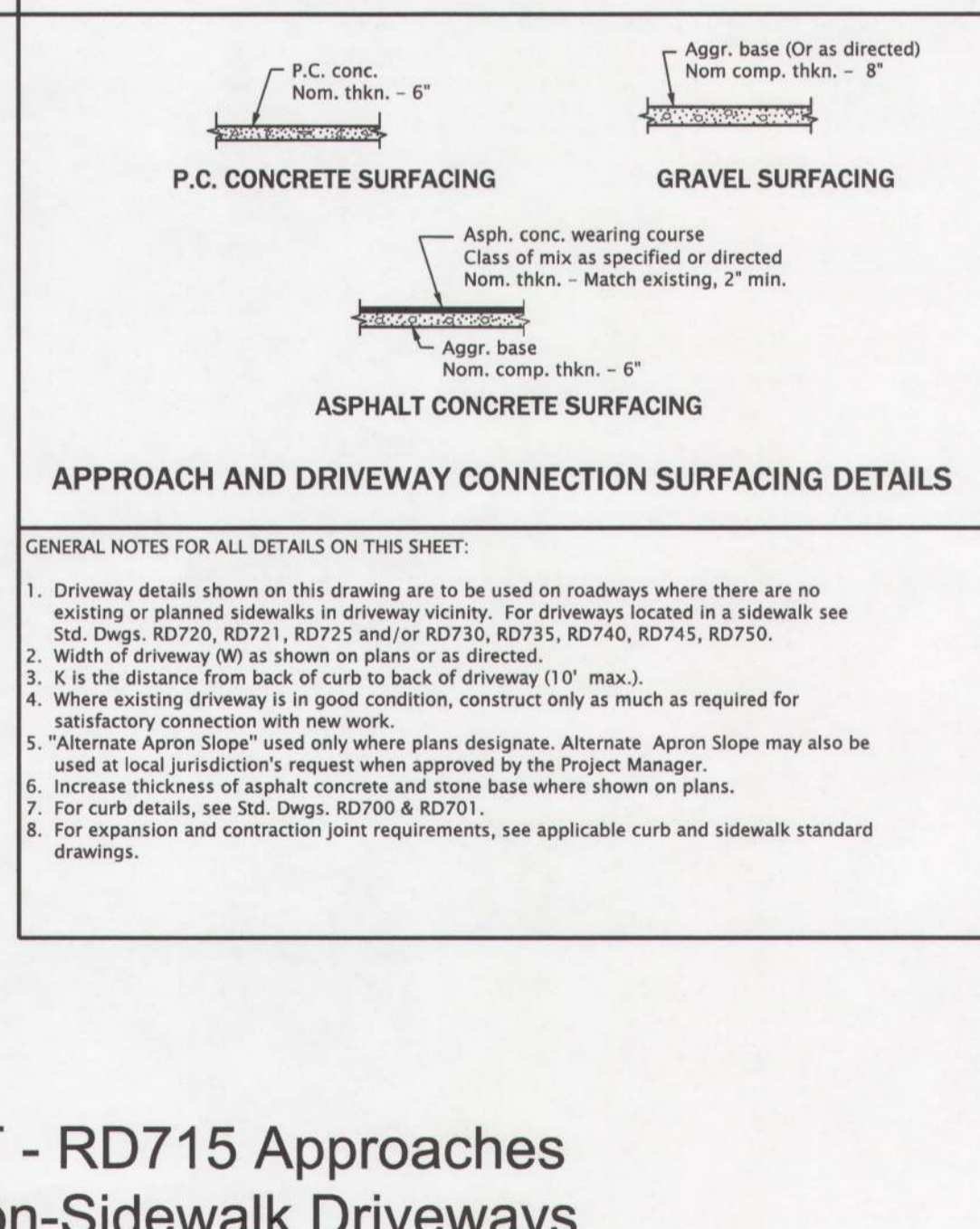


TABLE A

W (ft)	X (ft)	K (ft)			
		5	6	8	10
12		15	15	15	15
14		17	17	17	17
16	3	19	19	19	19
18		21	21	21	21
20		23	23	23	23
22		27	28	29	30
24		29	30	31	32
26		31	32	33	34
28		33	34	35	36
30		35	36	37	38
32	4	41	42	44	46
34		43	44	46	48
36	5	45	46	48	50

Where a travel lane is constructed adjacent to the curb line, use 16' W min. for residence and 30' W min. for light commercial, add 5' to W₁ for both. Do not add the 5' to W₁ when 4' min. shldr. or bikeway is included in the typical.



3
11 NTS
ODOT - RD715 Approaches and Non-Sidewalk Driveways

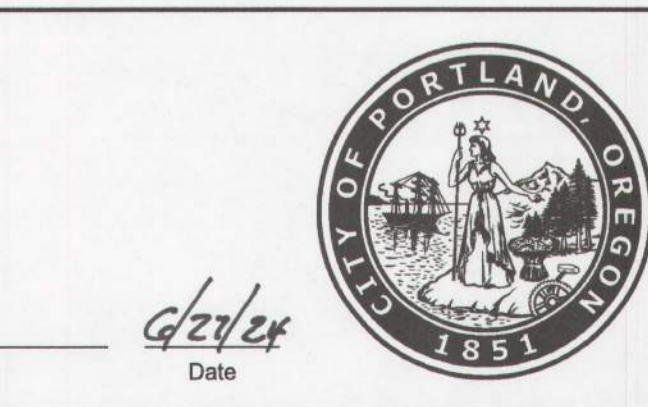
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CAD Checked By	GWE
Checked By	JDB
Project Mgr	KMA
Program Mgr	DMC
Const Mgr	BJN
Const Supvr	JDJ
Date	

REGISTERED PROFESSIONAL ENGINEER
19424
KENNETH MICHAEL ACKERMAN
EXPIRES 12-31-2025



Kenneth M. Ackerman, Principal Engineer, PE No 19424

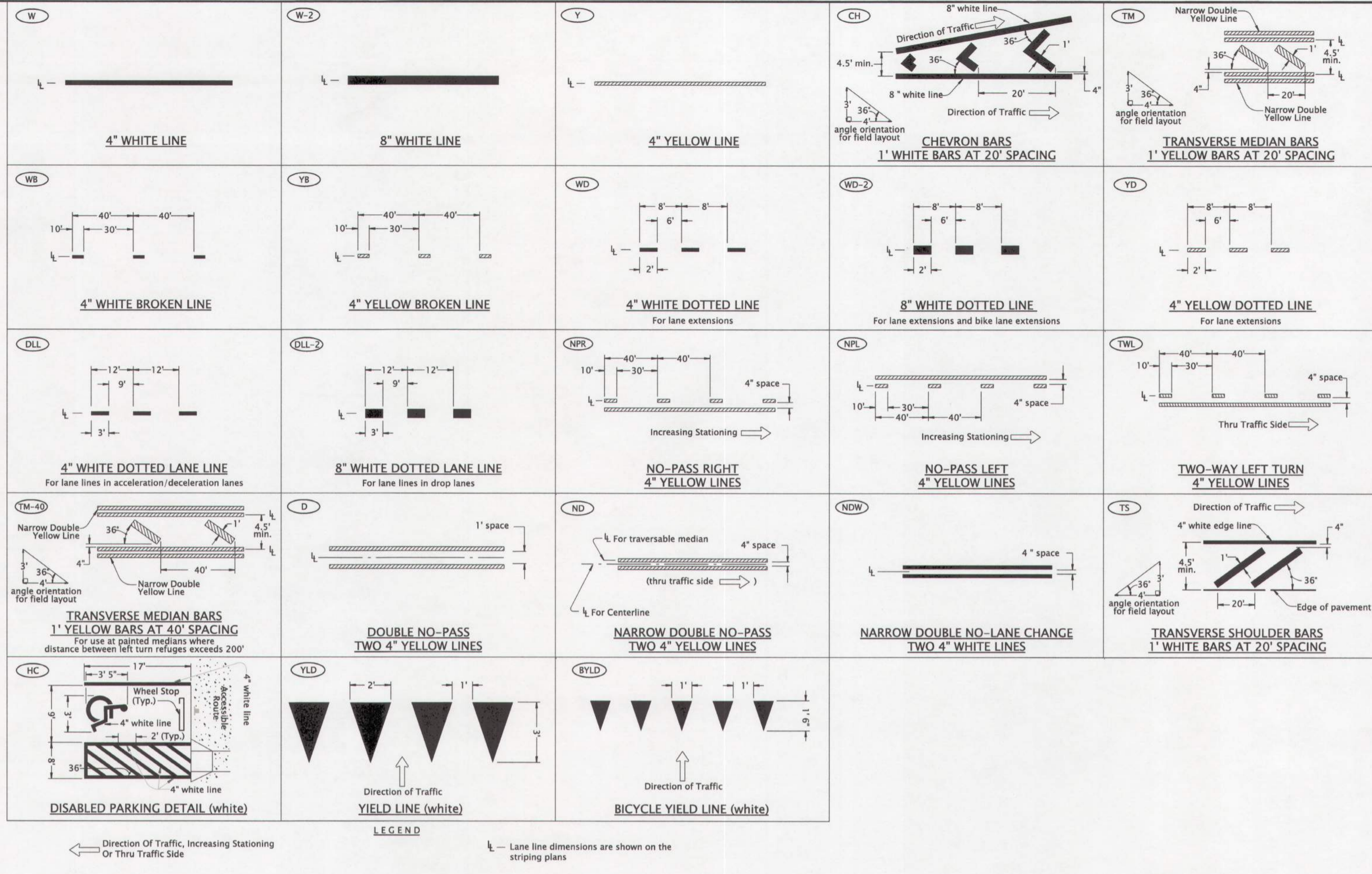


Pavement Rehabilitation Project
SE Altman Rd - SE Lusted Rd
SE Hosner Rd
ODOT Standard Drawings Sheet
RD610, RD615 & RD715

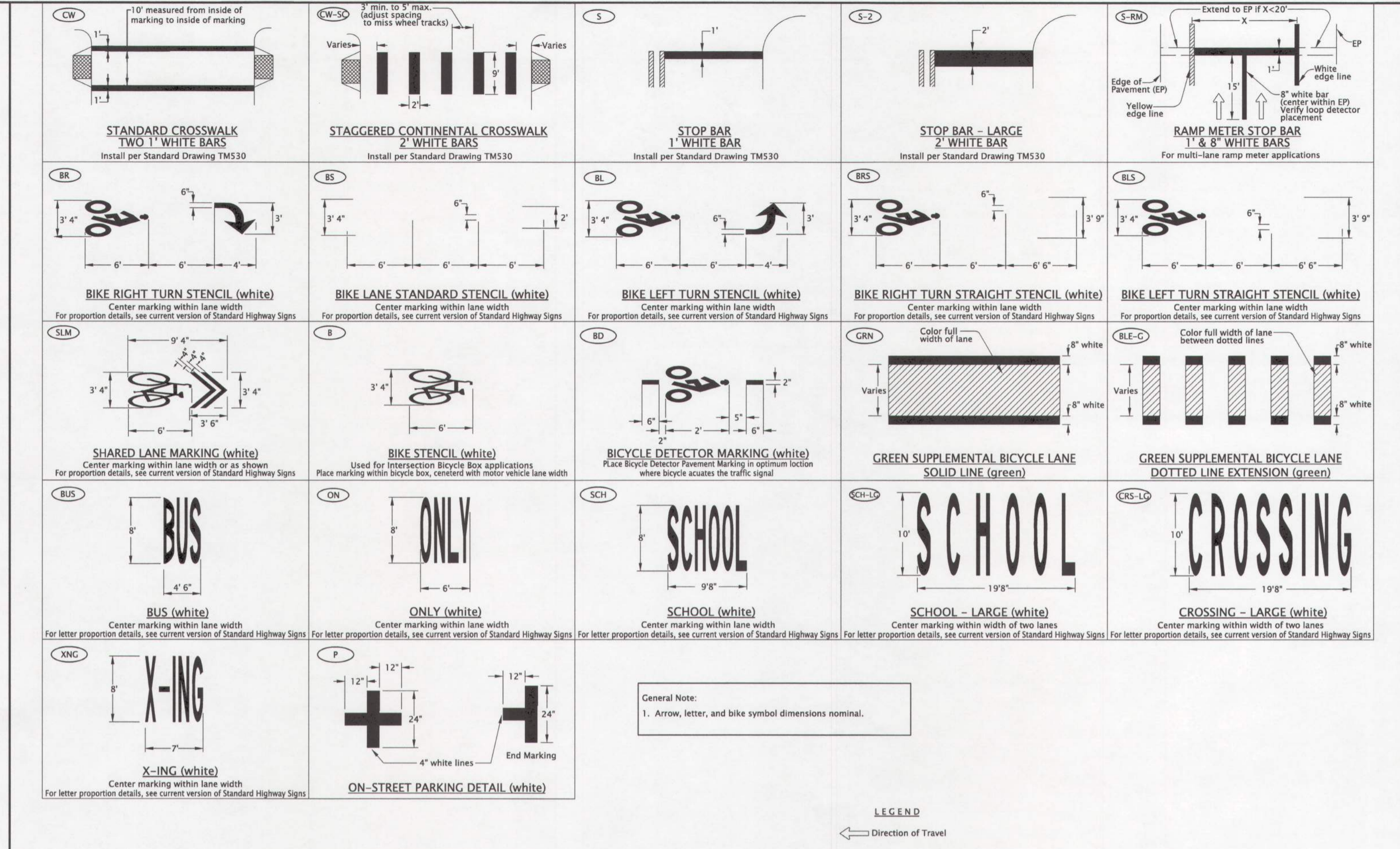
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3863, 3763, 3764,
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3666, 3564, 3565
Sheet No
11 of **24**

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6/26/2024

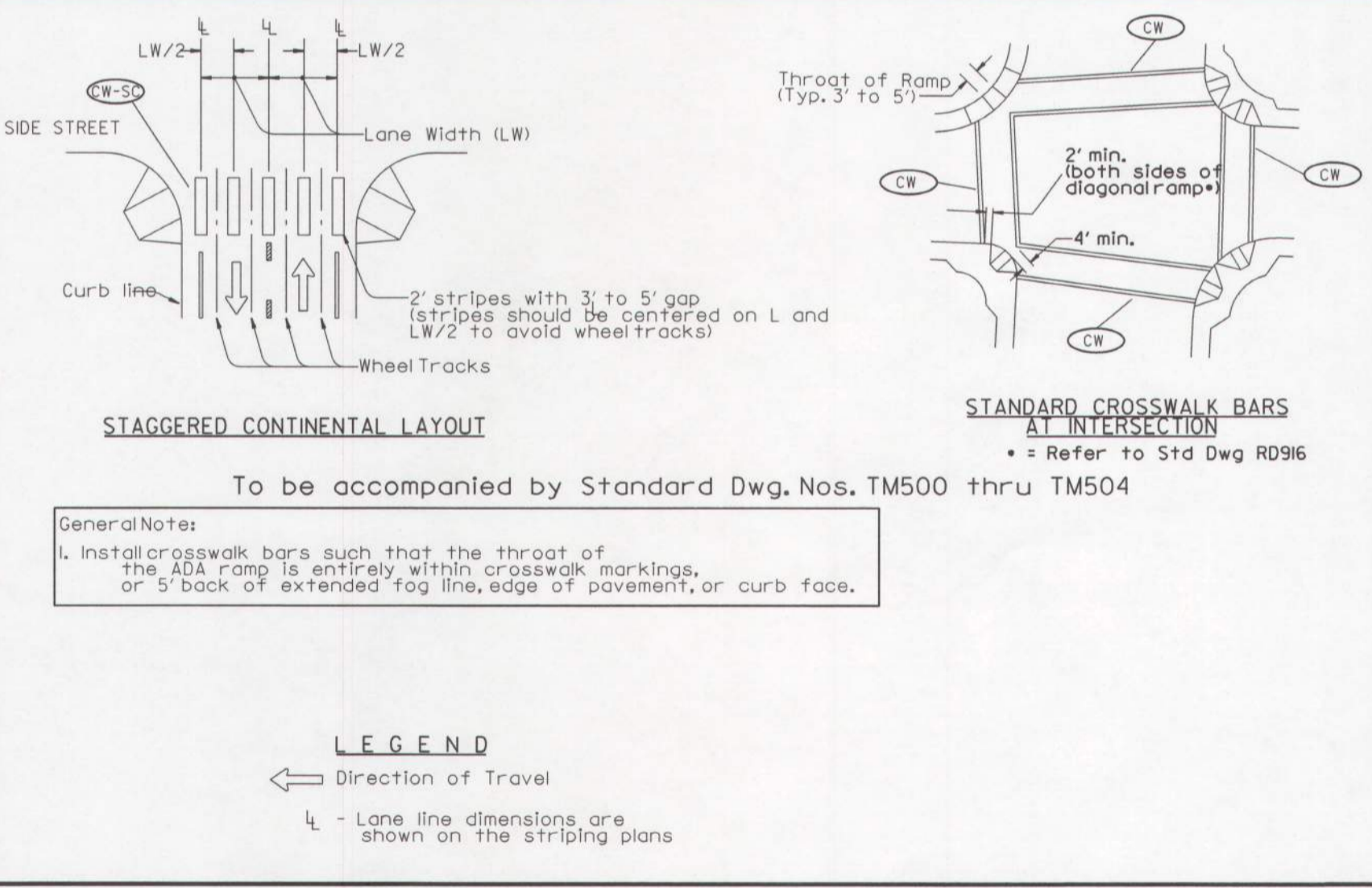
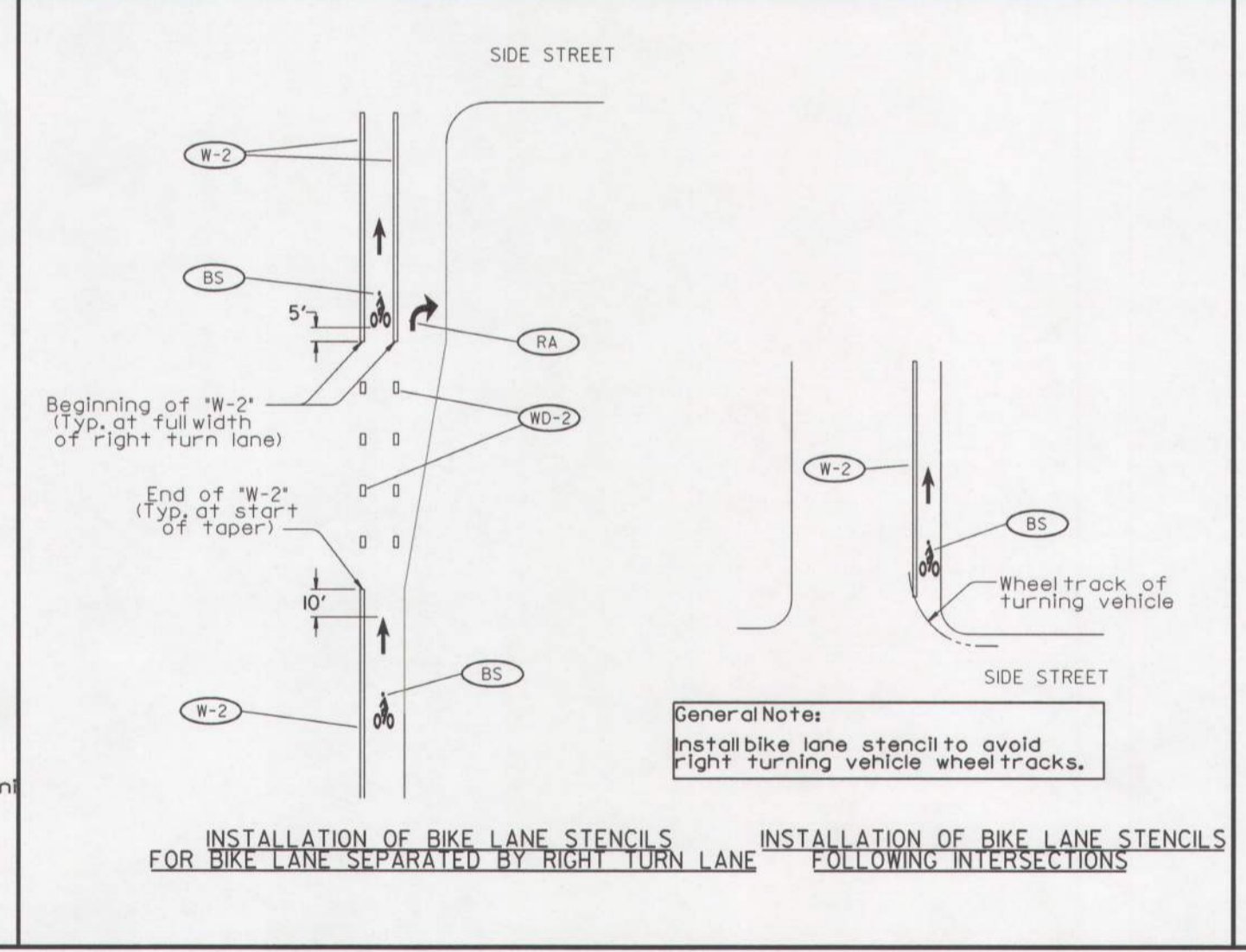
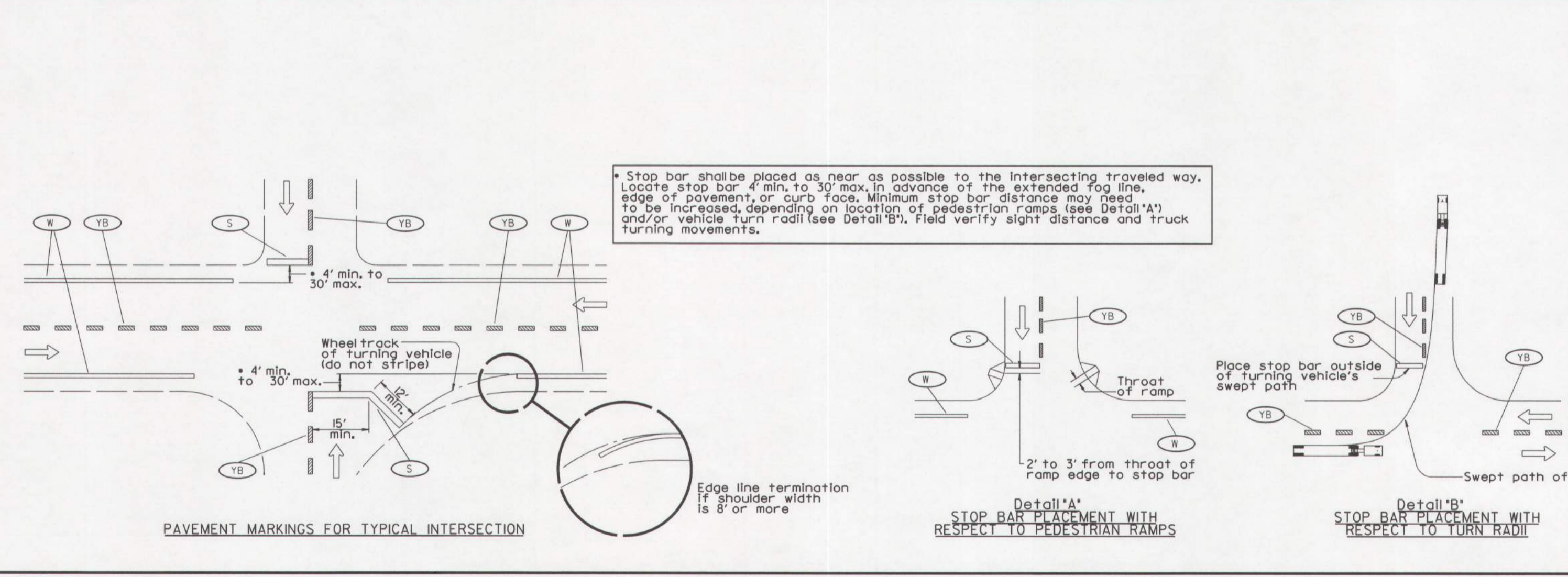
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1 ODOT - TM500 Pavement Marking Standard Detail Blocks
NTS



2 ODOT - TM503 Pavement Marking Standard Detail Blocks
NTS



3 ODOT - TM530 Intersection Pavement Markings
(Crosswalk, Stop Bar and Bike Lane Stencil)
NTS

No	Date	Description	Appd
Revision			
Survey	Unknown	Unknown	

Designed By	BJK	Program Mgr	DMC
CAD Checked By	gwe	Const Mgr	BJN
Checked By	JDB	Const Supr	JDT
Project Mgr	KMA	Date	

REGISTERED PROFESSIONAL ENGINEER
19424
KENNETH MICHAEL ACKERMAN
EXPIRES 12-31-2025



Kenneth M. Ackerman, Principal Engineer, PE No 19424
Date



Pavement Rehabilitation Project
SE Altman Rd - SE Lusted Rd
SE Hosner Rd
ODOT Standard Details Sheet
TM500, TM503 & TM530

SAP Project No
W02229
1/4 Section
3863, 3763, 3764,
3765, 3663, 3664,
3665, 3564, 3565
Sheet No
12 of 24

Not Used At This Time

No	Date	Description	Appd
Revision			
Survey	Unknown	Unknown	

Designed By BJK	Program Mgr <i>DMC</i>
CAD Checked By <i>QWE</i>	Const Mgr BJN
Checked By JDB	Const Supvr <i>JDJ</i>
Project Mgr KMA	Date



Kenneth M. Ackerman
Kenneth M. Ackerman, Principal Engineer, PE No 19424

6/27/24
Date

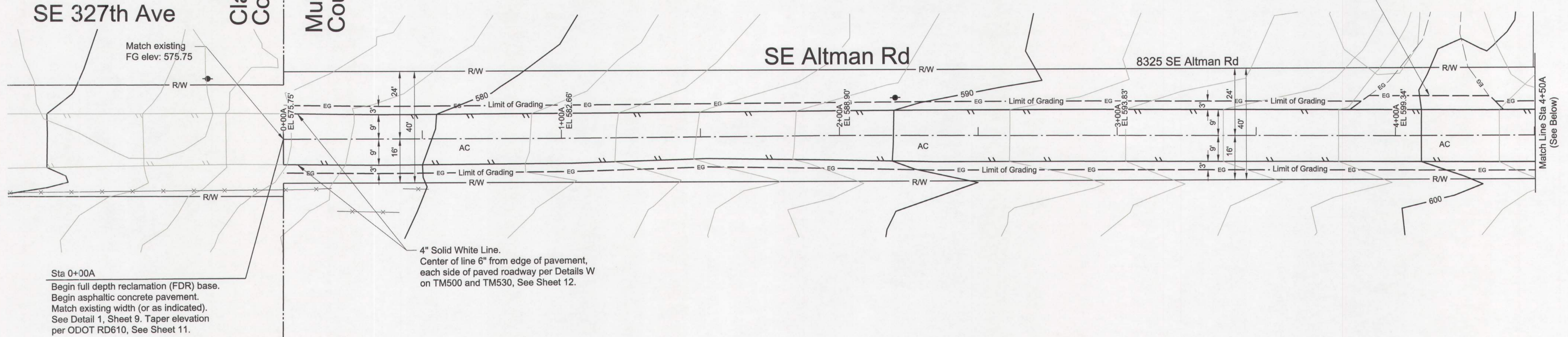


Pavement Rehabilitation Project
SE Altman Rd - SE Lusted Rd
SE Hosner Rd

SAP Project No
W02229
1/4 Section
3663, 3763, 3764,
3765, 3863, 3864,
3865, 3564, 3565
Sheet No

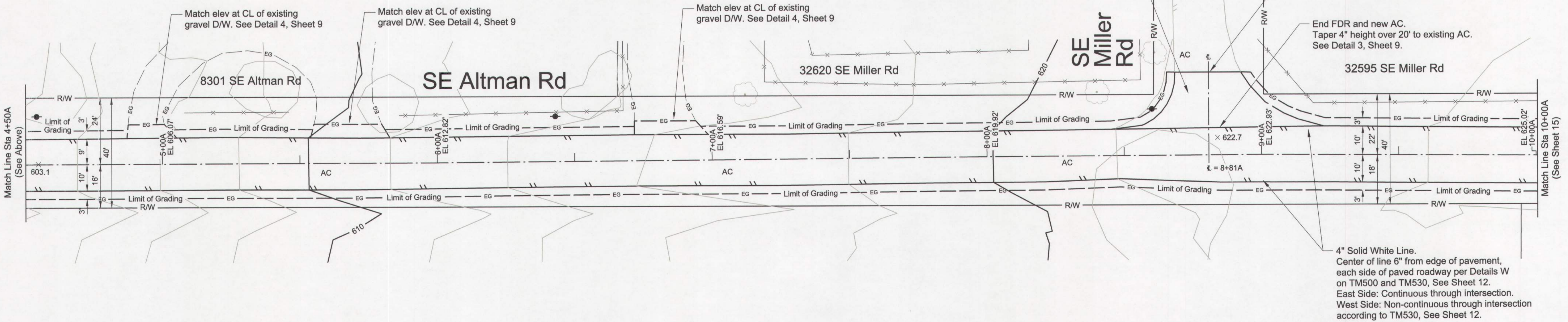
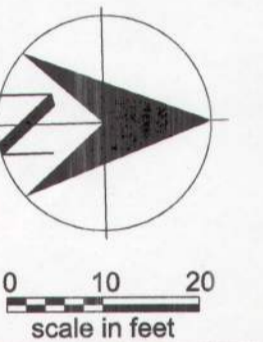
Notes:

- ① All existing signs, mailboxes, etc will not be removed and will be protected in place.
- ② Follow erosion control notes on Sheet 3 and Land Use permit.

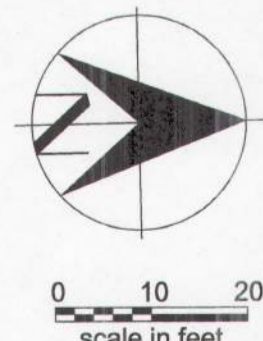


Sta 0+00A
 Begin full depth reclamation (FDR) base.
 Begin asphaltic concrete pavement.
 Match existing width (or as indicated).
 See Detail 1, Sheet 9. Taper elevation per ODOT RD610, See Sheet 11.

4" Solid White Line.
 Center of line 6" from edge of pavement,
 each side of paved roadway per Details W
 on TM500 and TM530, See Sheet 12.



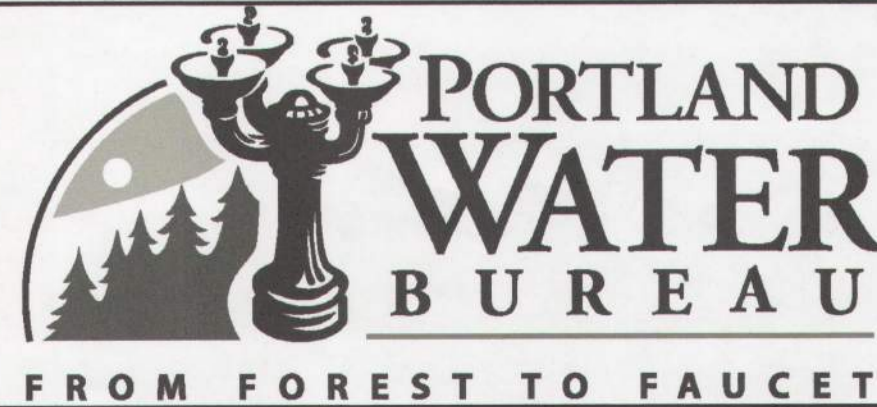
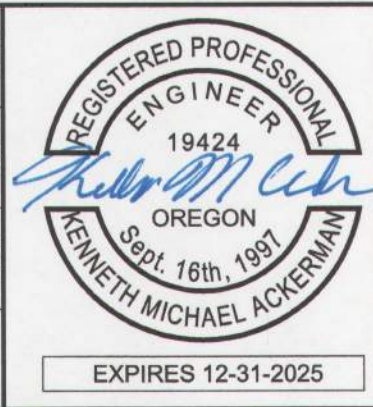
4" Solid White Line.
 Center of line 6" from edge of pavement,
 each side of paved roadway per Details W
 on TM500 and TM530, See Sheet 12.
 East Side: Continuous through intersection.
 West Side: Non-continuous through intersection
 according to TM530, See Sheet 12.



8/18/2024 11:48:01 AM J:\Engineering\CAD\Projects\W02229 - W02299\W02229 - Altman-Hosner-FDR\W02229.dgn

No	Date	Description	Appd

Designed By BJK	Program Mgr DMC
CAD Checked By GWE	Const Mgr BJN
Checked By JDB	Const Supvr JDJ
Project Mgr KMA	Date



Kenneth M. Ackerman
 Kenneth M. Ackerman, Principal Engineer, PE No 19424

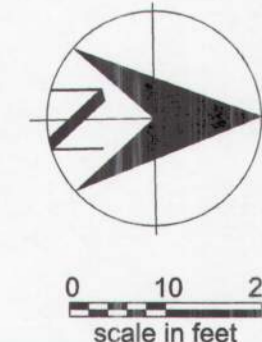
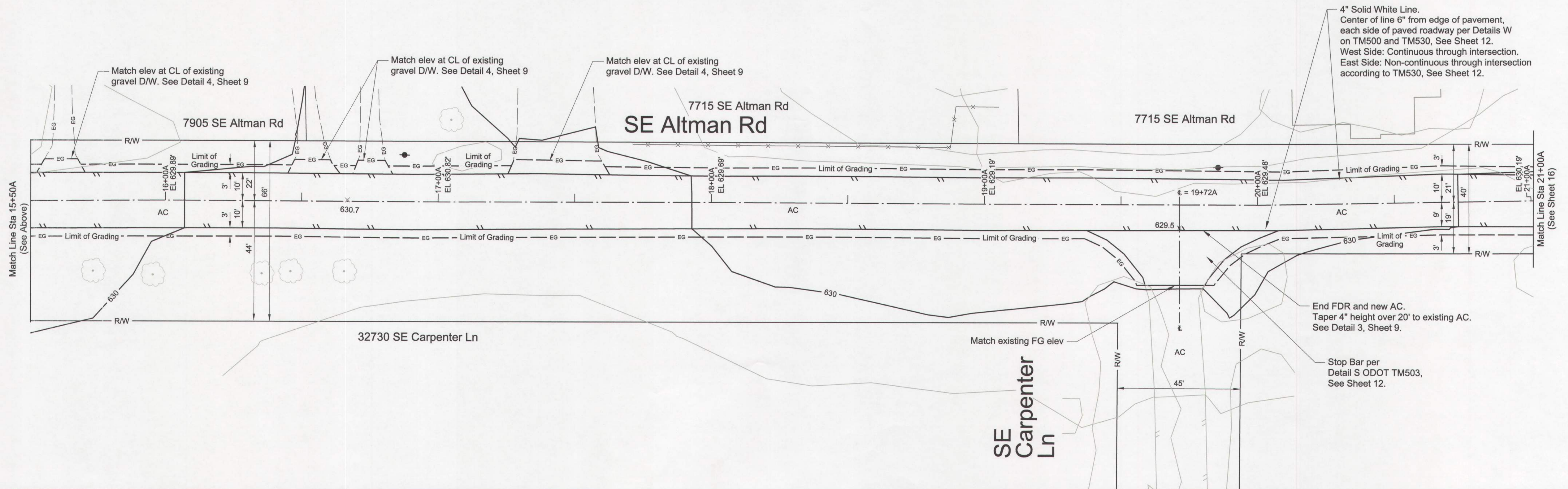
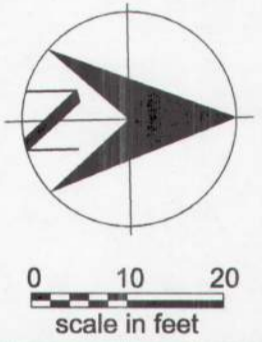
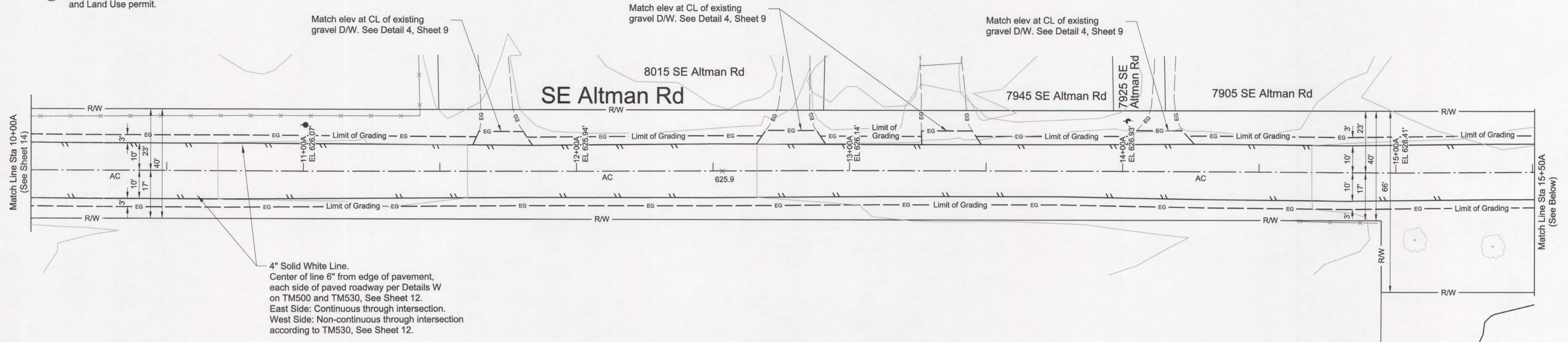


Pavement Rehabilitation Project
SE Altman Rd
 From County Line
 To SE Lusted Rd
 Plan Sheet
 Sta 0+00A to Sta 10+00A

SAP Project No W02229
1/4 Section 3963, 3863
Sheet No 14 of 24

Notes:

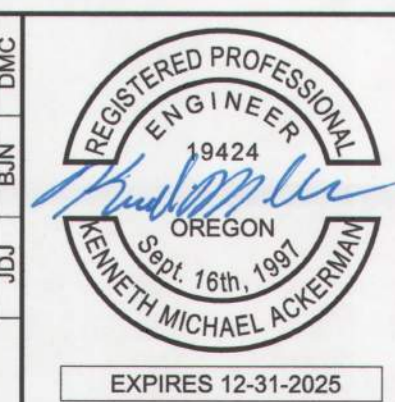
- ① All existing signs, mailboxes, etc will not be removed and will be protected in place.
- ② Follow erosion control notes on Sheet 3 and Land Use permit.



8/18/2024 11:48:09 AM J:\Engineering\CAD\Projects\W02229 - W02298\W02229_Altman-Hosmer-FDR\W02229.dgn

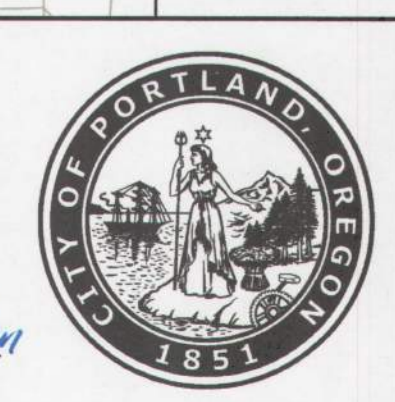
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Survey	Unknown	U-known	

Designed By BJK	Program Mgr DMC
CAD Checked By gwe	Const Mgr BJN
Checked By JDB	Const Supvr JDT
Project Mgr KMA	Date



Kenneth M. Ackerman
Kenneth M. Ackerman, Principal Engineer, PE No 19424

E. Liden
Date



Pavement Rehabilitation Project

SE Altman Rd

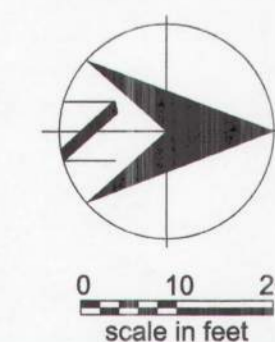
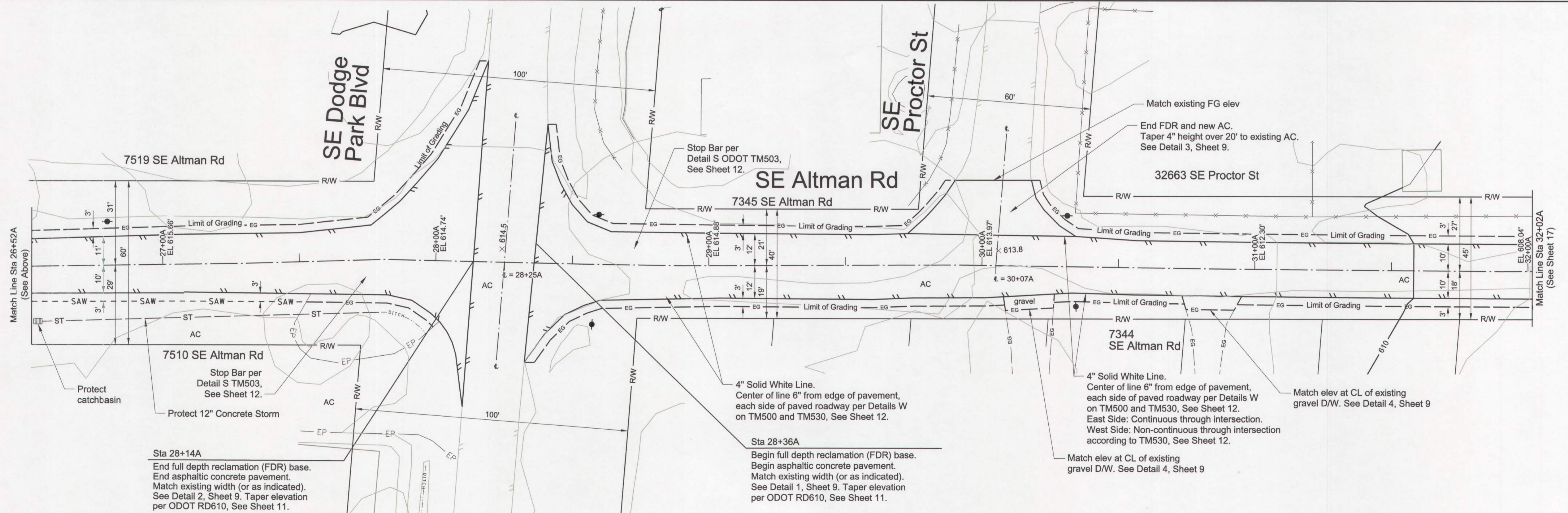
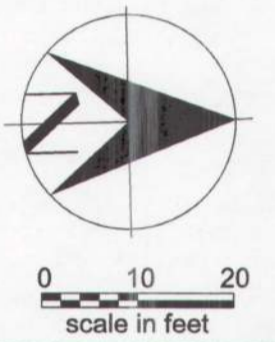
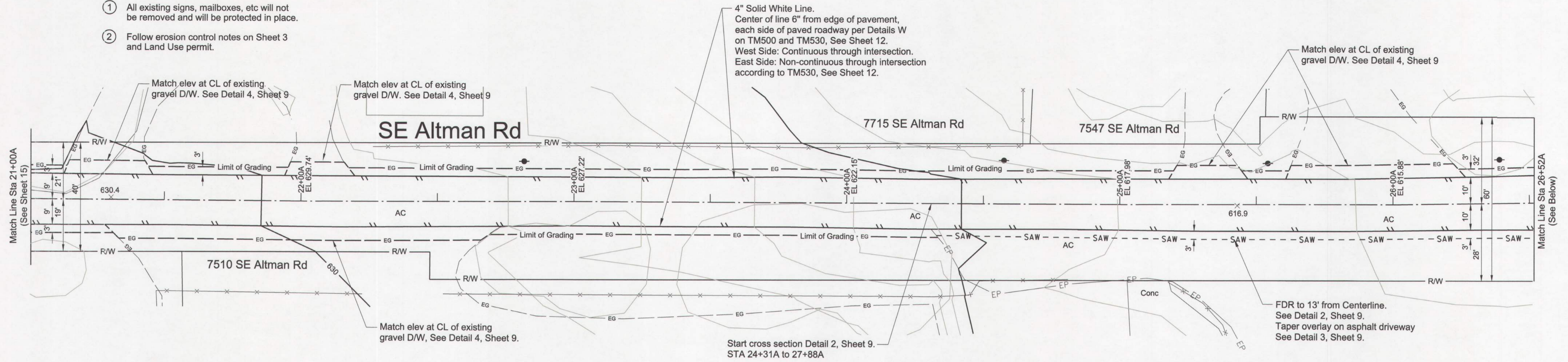
From County Line
To SE Lusted Rd

Plan Sheet
Sta 10+00A to Sta 21+00A

SAP Project No W02229
1/4 Section 3863
Sheet No 15 of 24

Notes:

- ① All existing signs, mailboxes, etc will not be removed and will be protected in place.
- ② Follow erosion control notes on Sheet 3 and Land Use permit.



8/18/2024 11:48:16 AM J:\Engineering\CAD\Projects\W022200 - W022298\W02229 - Altman-Hosmer-FDR\W02229.dgn

No	Date	Description	Appd
Revision			
Survey	Unknown	U-known	

Designed By	Program Mgr
BJK	DMC
CAD Checked By	Const Mgr
GWE	BJN
Checked By	Const Supvr
JDB	JDT
Project Mgr	Date
KMA	

REGISTERED PROFESSIONAL ENGINEER
19424
Kenneth M. Ackerman
OREGON
EXPIRES 12-31-2025



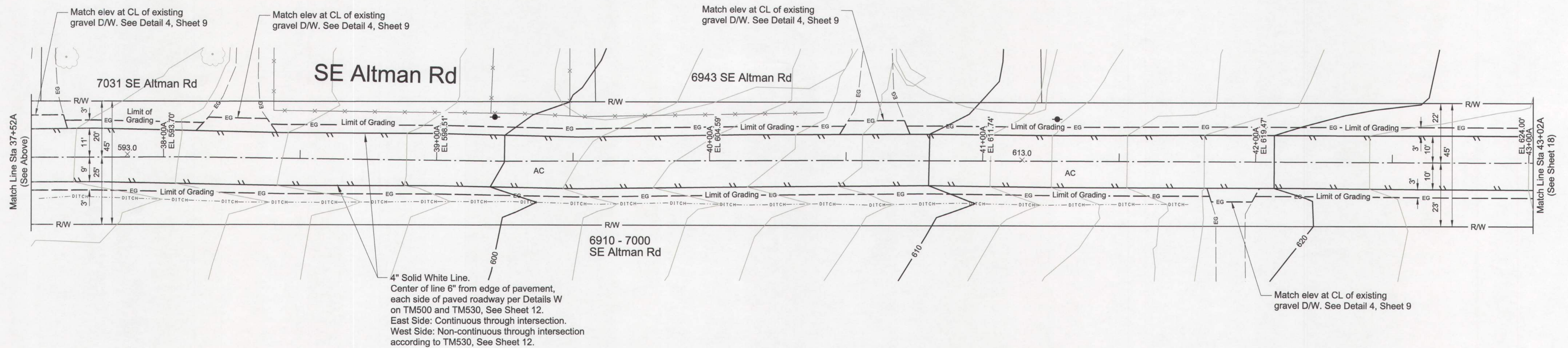
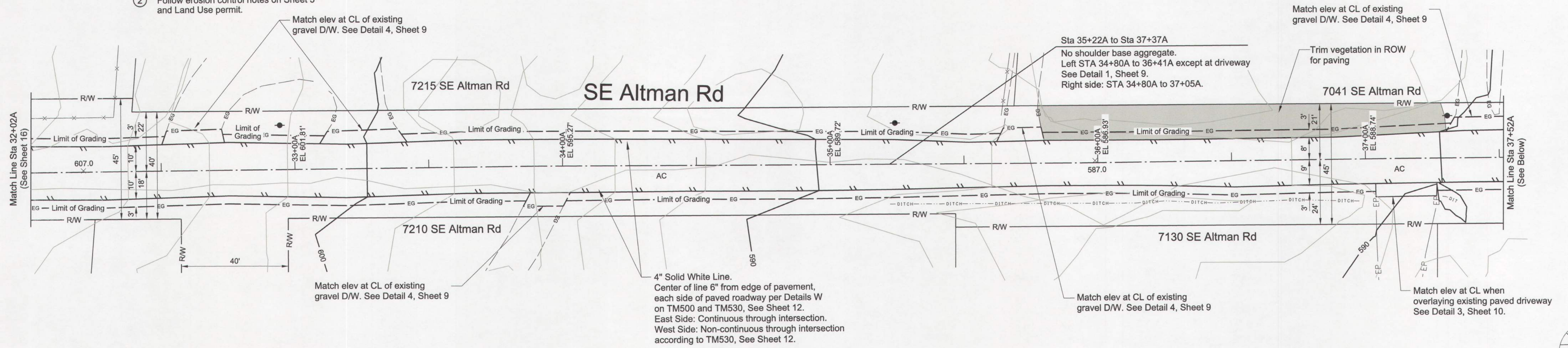
Kenneth M. Ackerman
Kenneth M. Ackerman, Principal Engineer, PE No 19424
Date: 8/19/24

Pavement Rehabilitation Project
SE Altman Rd
From County Line To SE Lusted Rd
Plan Sheet
Sta 21+00A to Sta 32+02A

SAP Project No
W02229
1/4 Section
3863, 3763
Sheet No
16 of 24

Notes:

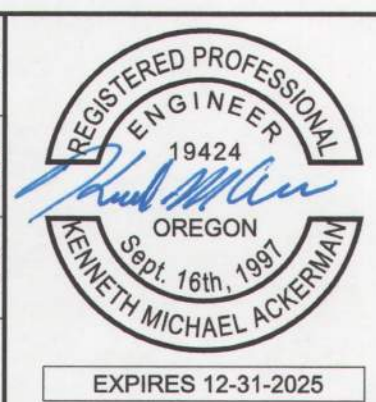
- ① All existing signs, mailboxes, etc will not be removed and will be protected in place.
- ② Follow erosion control notes on Sheet 3 and Land Use permit.



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No	Date	Description	Appd
Revision			
Survey	Unknown	Unknown	

Designed By BJK	Program Mgr DMC
CAD Checked By GWE	Const Mgr BJN
Checked By JDB	Const Supvr JDT
Project Mgr KMA	Date



Kenneth M. Ackerman
Kenneth M. Ackerman, Principal Engineer, PE No 19424
8/16/24
Date

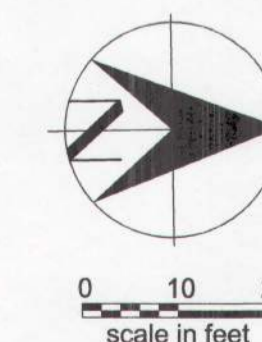
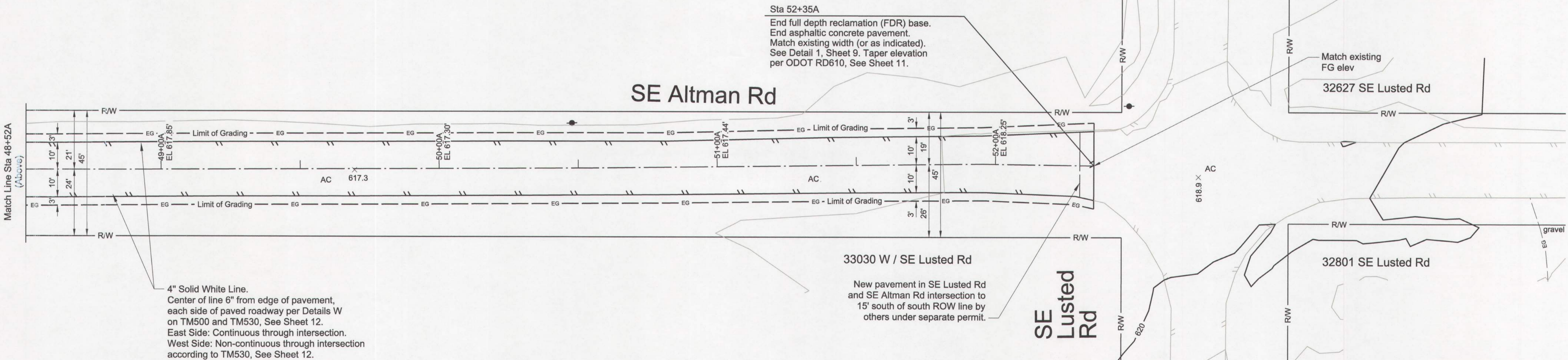
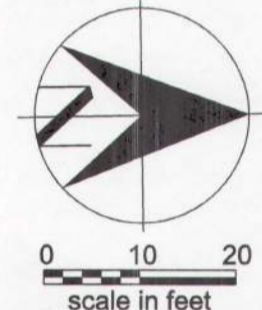
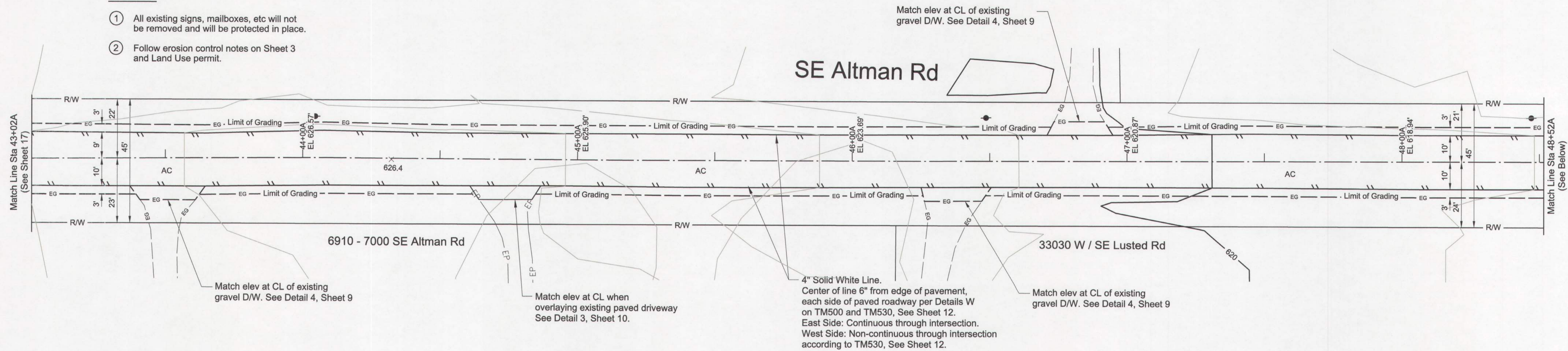


Pavement Rehabilitation Project
SE Altman Rd
From County Line
To SE Lusted Rd
Plan Sheet
Sta 32+02A to Sta 43+02A

SAP Project No
W02229
1/4 Section
3763
Sheet No
17 of 24

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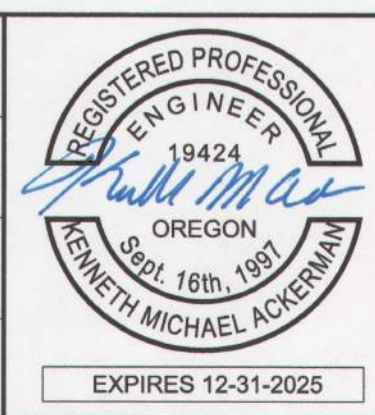
- ① All existing signs, mailboxes, etc will not be removed and will be protected in place.
- ② Follow erosion control notes on Sheet 3 and Land Use permit.



8/18/2024 11:48:31 AM J:\Engineering\CAD\Projects\W02229 - W02299\W02229 - Altman-Hosner-FDR\W02229.dgn

No	Date	Description	Appd
Revision			

Designed By BJK	Program Mgr DMC
CAD Checked By GWE	Const Mgr BJN
Checked By JDB	Const Supvr JDT
Project Mgr KMA	Date



Kenneth M. Ackerman
Kenneth M. Ackerman, Principal Engineer, PE No 19424
8/14/24
Date

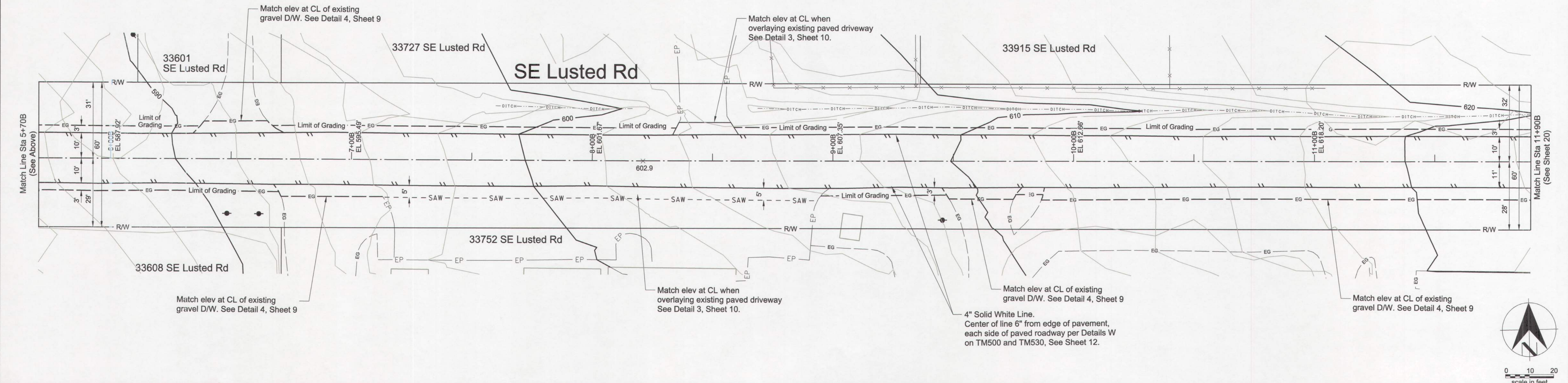
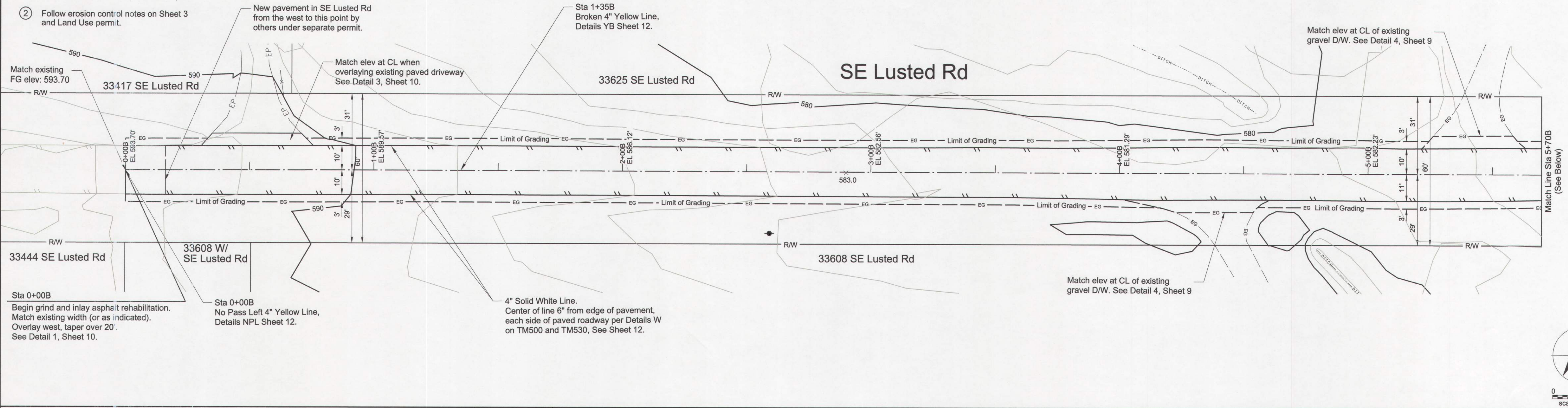


Pavement Rehabilitation Project
SE Altman Rd
From County Line
To SE Lusted Rd
Plan Sheet
Sta 43+02A to Sta 52+35A

SAP Project No W02229
1/4 Section 3863, 3763, 3764, 3763, 3663
Sheet No 18 of 24

Notes:

- ① All existing signs, mailboxes, etc will not be removed and will be protected in place.
- ② Follow erosion control notes on Sheet 3 and Land Use permit.



8/18/2024 11:48:38 AM J:\Engineering\CAD\Projects\W02229 - W02299\W02229_Altman-Hosner-FDR\W02229.dgn

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Revision			
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Designed By BJK	Program Mgr DMC
CAD Checked By GWE	Const Mgr BJN
Checked By JDB	Const Supvr JDT
Project Mgr KMA	Date



Kenneth M. Ackerman
Kenneth M. Ackerman, Principal Engineer, PE No 19424

8/19/24
Date



Pavement Rehabilitation Project

SE Lusted Rd

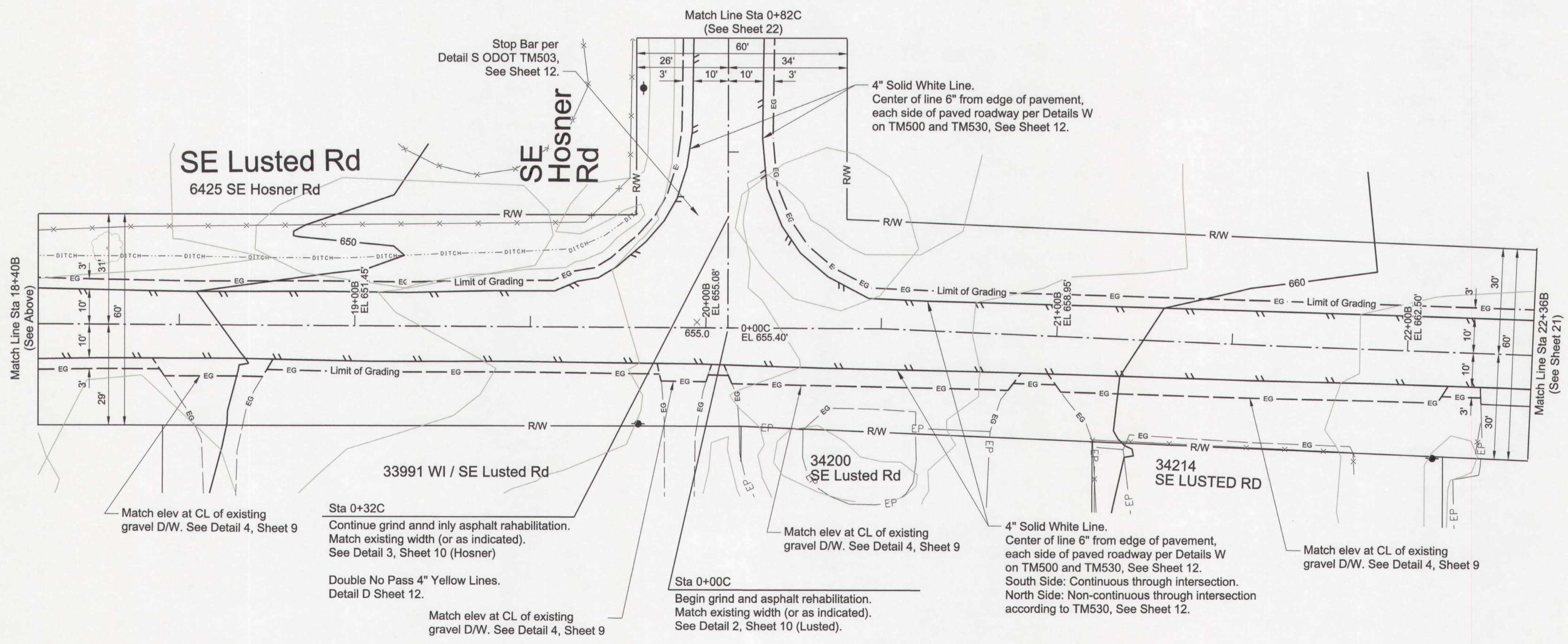
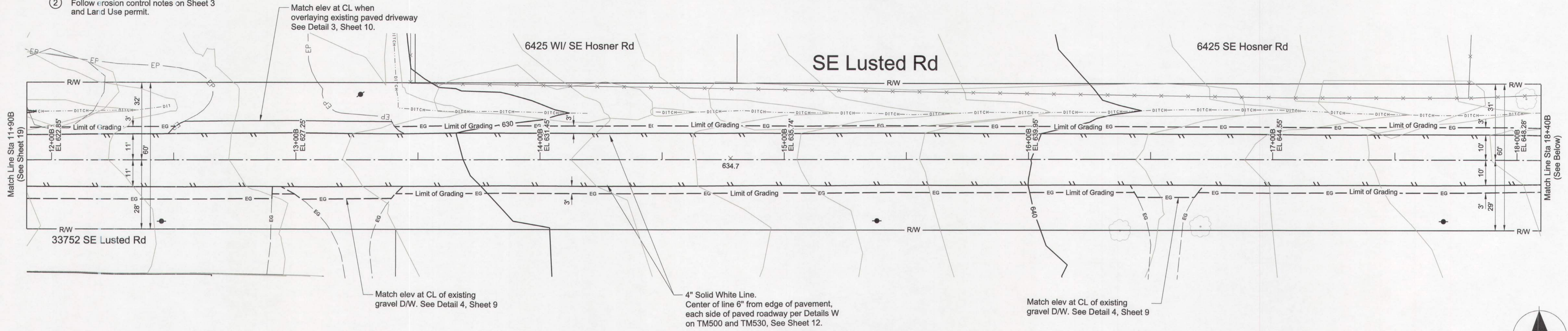
From 33444 SE Lusted Rd
To SE Cottrell Rd

Plan Sheet
Sta 0+00B to Sta 11+90B

SAP Project No W02229
1/4 Section 3764, 3664
Sheet No 19 of 24

Notes:

- ① All existing signs, mailboxes, etc will not be removed and will be protected in place.
- ② Follow erosion control notes on Sheet 3 and Land Use permit.



8/18/2024 11:48:46 AM J:\Engineering\CAD\Projects\W022200 - W02299\W02229 - Altman-Hosner-FDR\W02229.dgn

No	Date	Description	Appd
Revision			
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Designed By
BJK

CAD Checked By
GWE

Checked By
JDB

Project Mgr
KMA

Program Mgr
DMC

Const Mgr
BJN

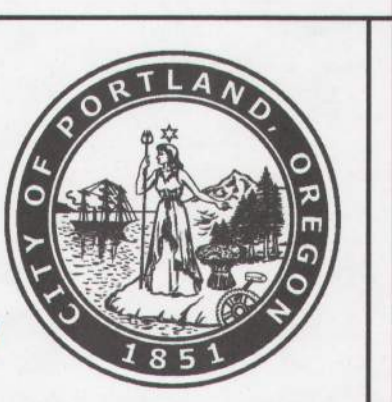
Const Supvr
JDT

Date



Kenneth M. Ackerman
Kenneth M. Ackerman, Principal Engineer, PE No 19424

8/17/24
Date



Pavement Rehabilitation Project

SE Lusted Rd

From 33444 SE Lusted Rd
To SE Cottrell Rd

Plan Sheet
Sta 11+90B to Sta 22+36B / Sta 0+00C to 0+82C

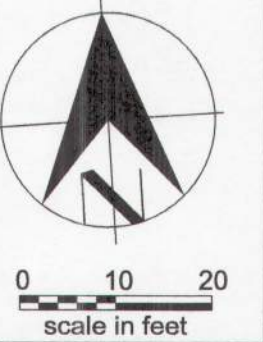
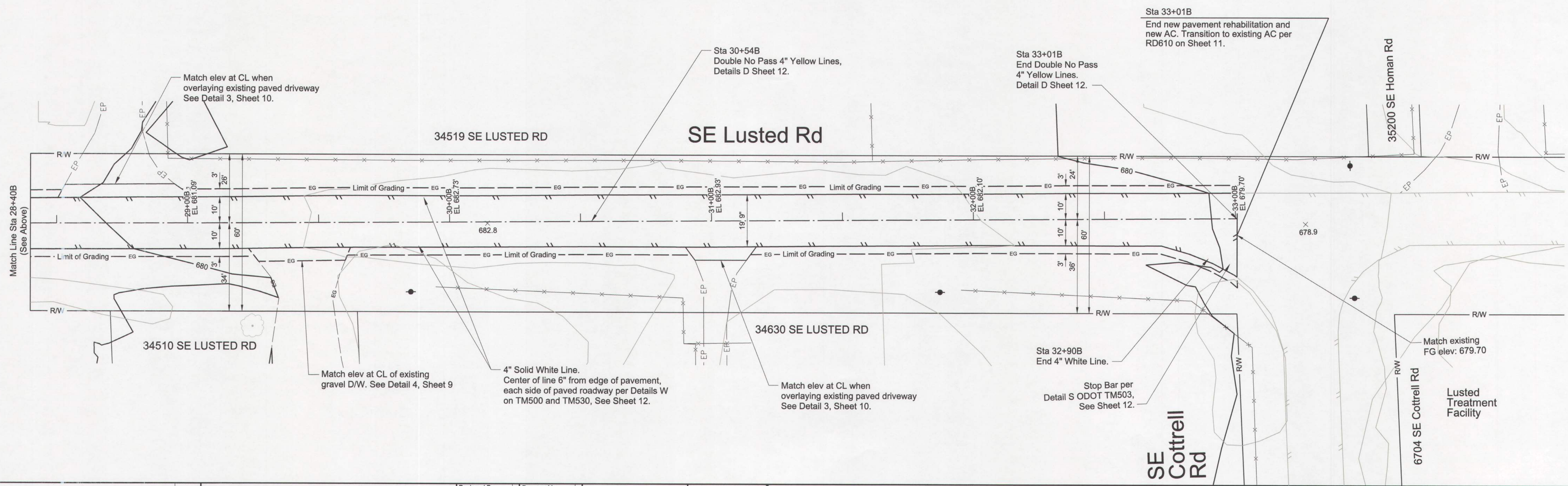
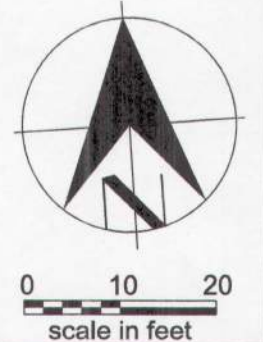
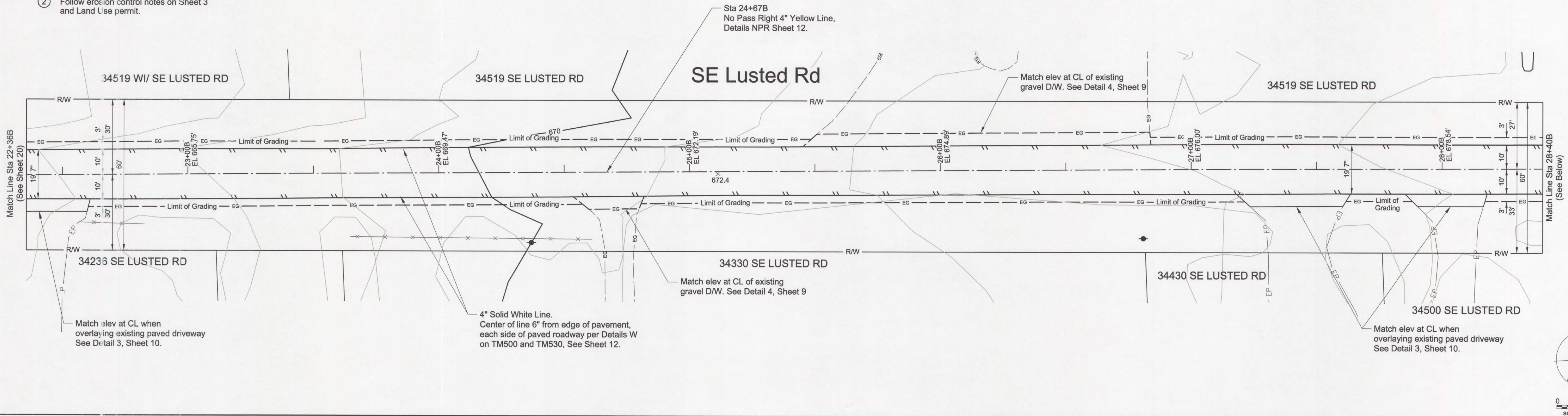
SAP Project No
W02229

1/4 Section
3764, 3664

Sheet No
20 of 24

Notes:

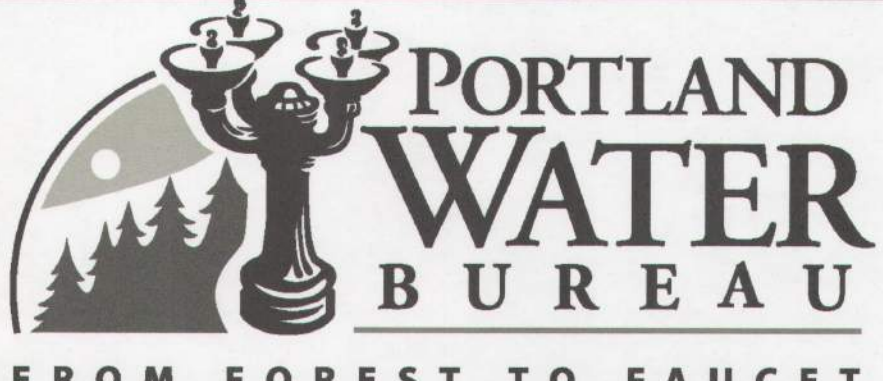
- ① All existing signs, mailboxes, etc will not be removed and will be protected in place.
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No	Date	Description	Appd

Designed By BJK	Program Mgr DMC
CAD Checked By QWE	Const Mgr BJN
Checked By JDB	Const Supvr JDT
Project Mgr KMA	Date



Kenneth M. Ackerman
Kenneth M. Ackerman, Principal Engineer, PE No 19424

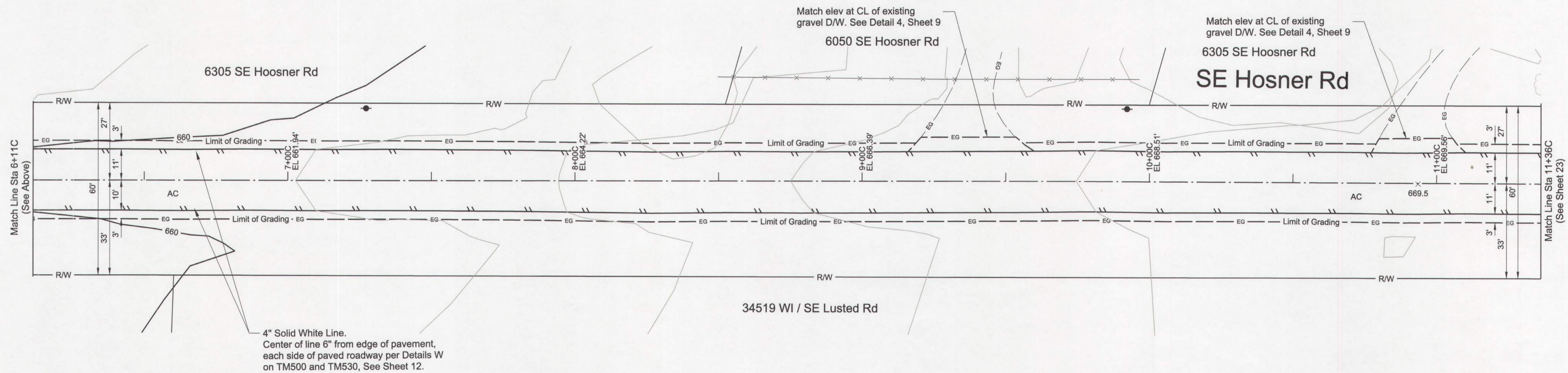
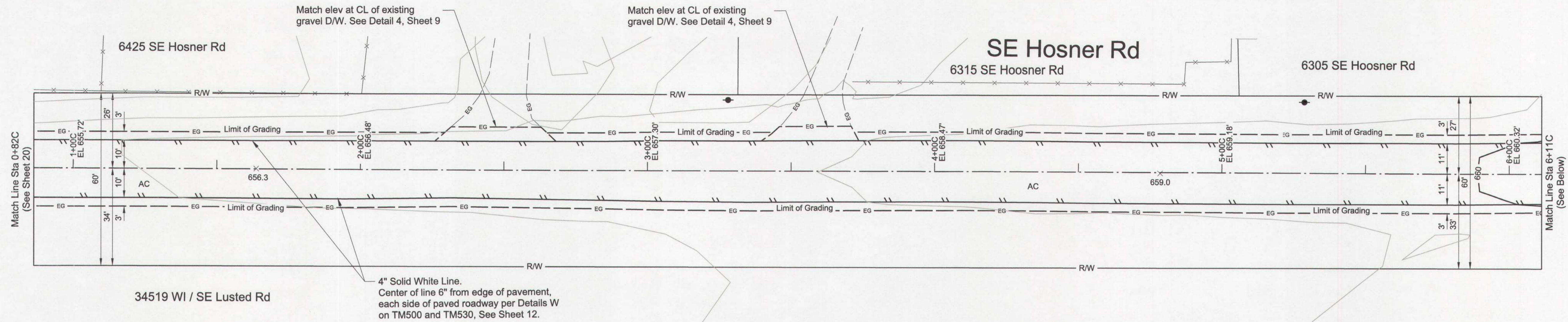


Pavement Rehabilitation Project
SE Lusted Rd
From 33444 SE Lusted Rd
To SE Cottrell Rd
Plan Sheet
Sta 22+36B to Sta 33+01B

SAP Project No W02229
1/4 Section 3665, 3765
Sheet No 21 of 24

Notes:

- ① All existing signs, mailboxes, etc will not be removed and will be protected in place.
- ② Follow erosion control notes on Sheet 3 and Land Use permit.



8/18/2024 11:49:01 AM J:\Engineering\CAD\Projects\W022200 - W02299\W02229 - Altman-Hosner-FDR\W02229.dgn

No	Date	Description	Appd
Revision			
Survey	Unknown	U-known	

Designed By BJK	Program Mgr DMC
CAD Checked By gwe	Const Mgr BJN
Checked By JDB	Const Supvr JDT
Project Mgr KMA	Date



Kenneth M. Ackerman
Kenneth M. Ackerman, Principal Engineer, PE No 19424

8/19/24
Date



Pavement Rehabilitation Project

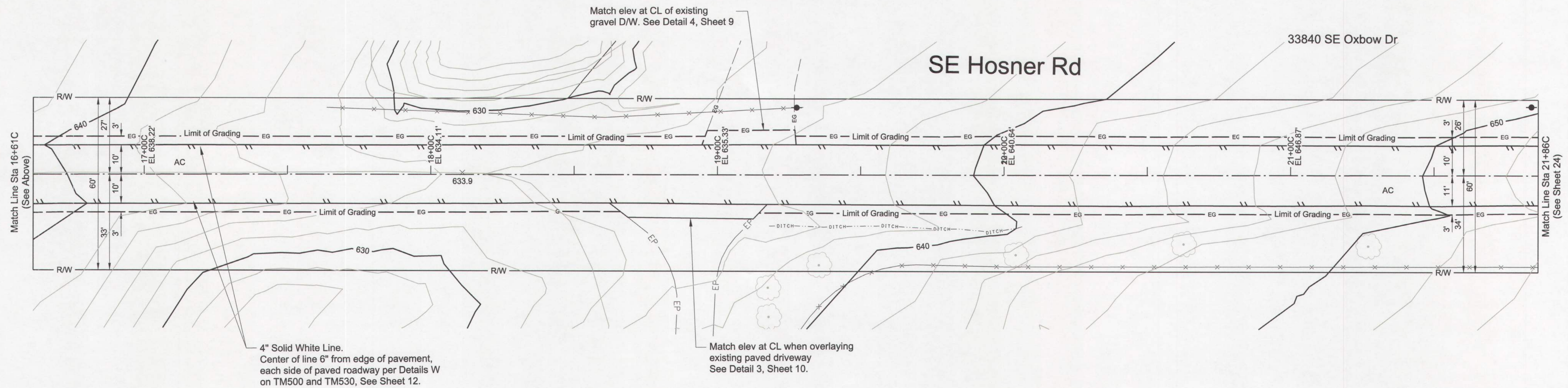
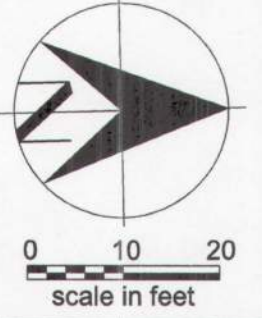
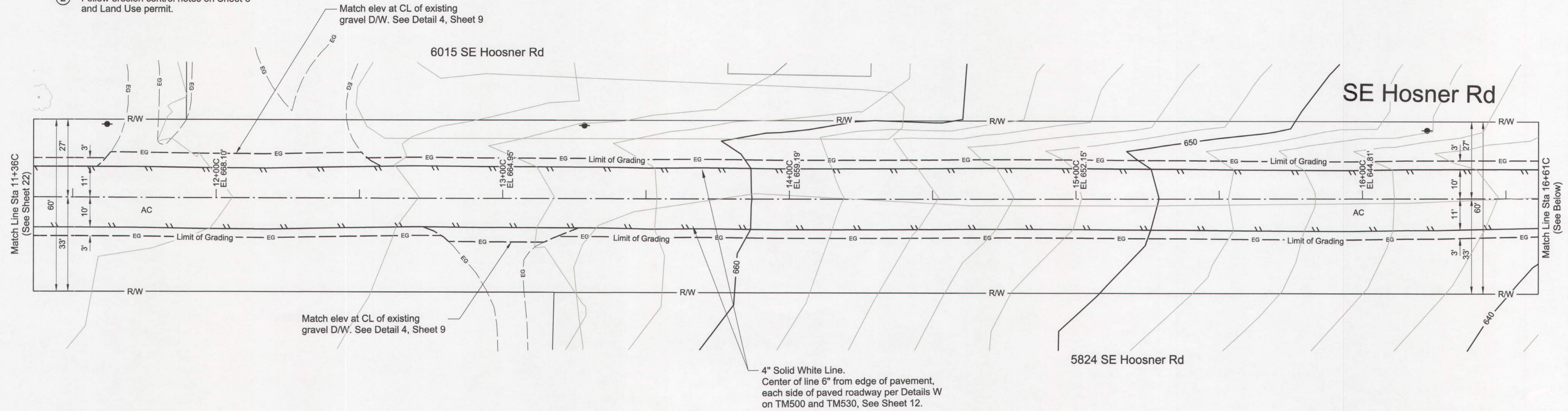
SE Hosner Rd

From SE Lusted Rd
To SE Oxbow Dr
Plan Sheet
Sta 0+82C to Sta 11+36C

SAP Project No W02229
1/4 Section 3664, 3665
Sheet No 22 of 24

Notes:

- ① All existing signs, mailboxes, etc will not be removed and will be protected in place.
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8/18/2024 11:49:08 AM J:\Engineering\CAD\Projects\W02229 - W02299\W02229 - Altman-Hosner-FDR\W02229.dgn

No	Date	Description	Appd
Revision			
Survey	Unknown	U-known	

Designed By BJK	Program Mgr DMC
CAD Checked By GWE	Const Mgr BJN
Checked By JDB	Const Supvr JDT
Project Mgr KMA	Date



Kenneth M. Ackerman
Kenneth M. Ackerman, Principal Engineer, PE No 19424
Date: *8/18/24*



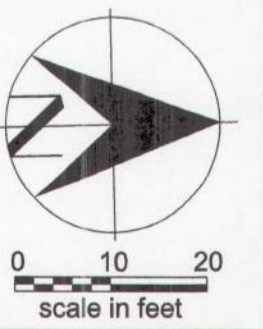
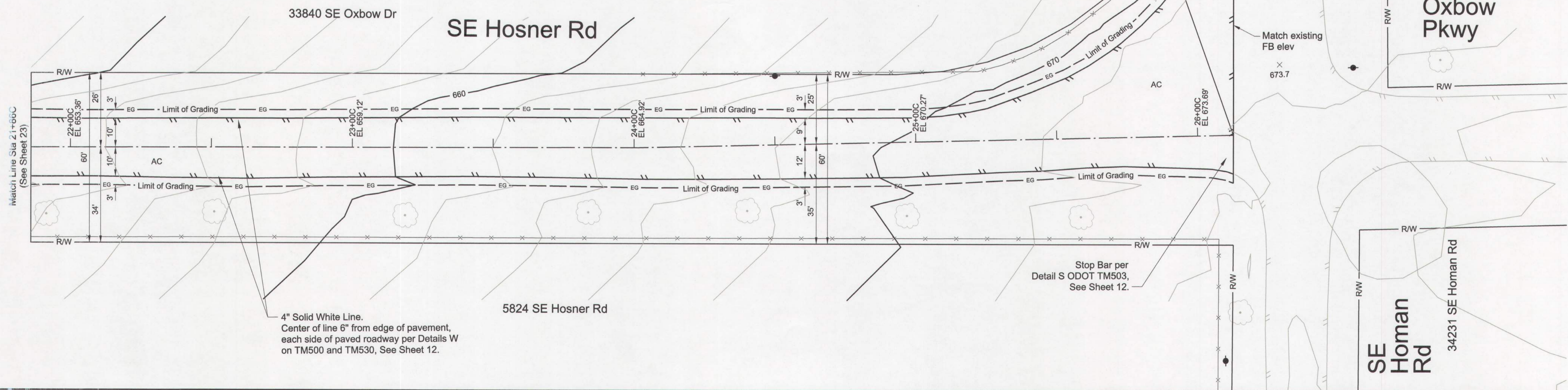
Pavement Rehabilitation Project
SE Hosner Rd
From SE Lusted Rd
To SE Oxbow Dr
Plan Sheet
Sta 11+36C to Sta 21+86C

SAP Project No W02229
1/4 Section 3664, 3665
Sheet No 23 of 24

8/18/2024 11:49:15 AM J:\Engineering\CAD\Projects\W02200 - W02229\W02229_Altman-Hosner-FDR\W02229.dgn

Notes:

- ① All existing signs, mailboxes, etc will not be removed and will be protected in place.
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No	Date	Description	Appd
Revision			
Survey	Unknown	Unknown	

Designed By BJK	Program Mgr DMC
CAD Checked By GWE	Const Mgr BJN
Checked By JDB	Const Supvr JDT
Project Mgr KMA	Date



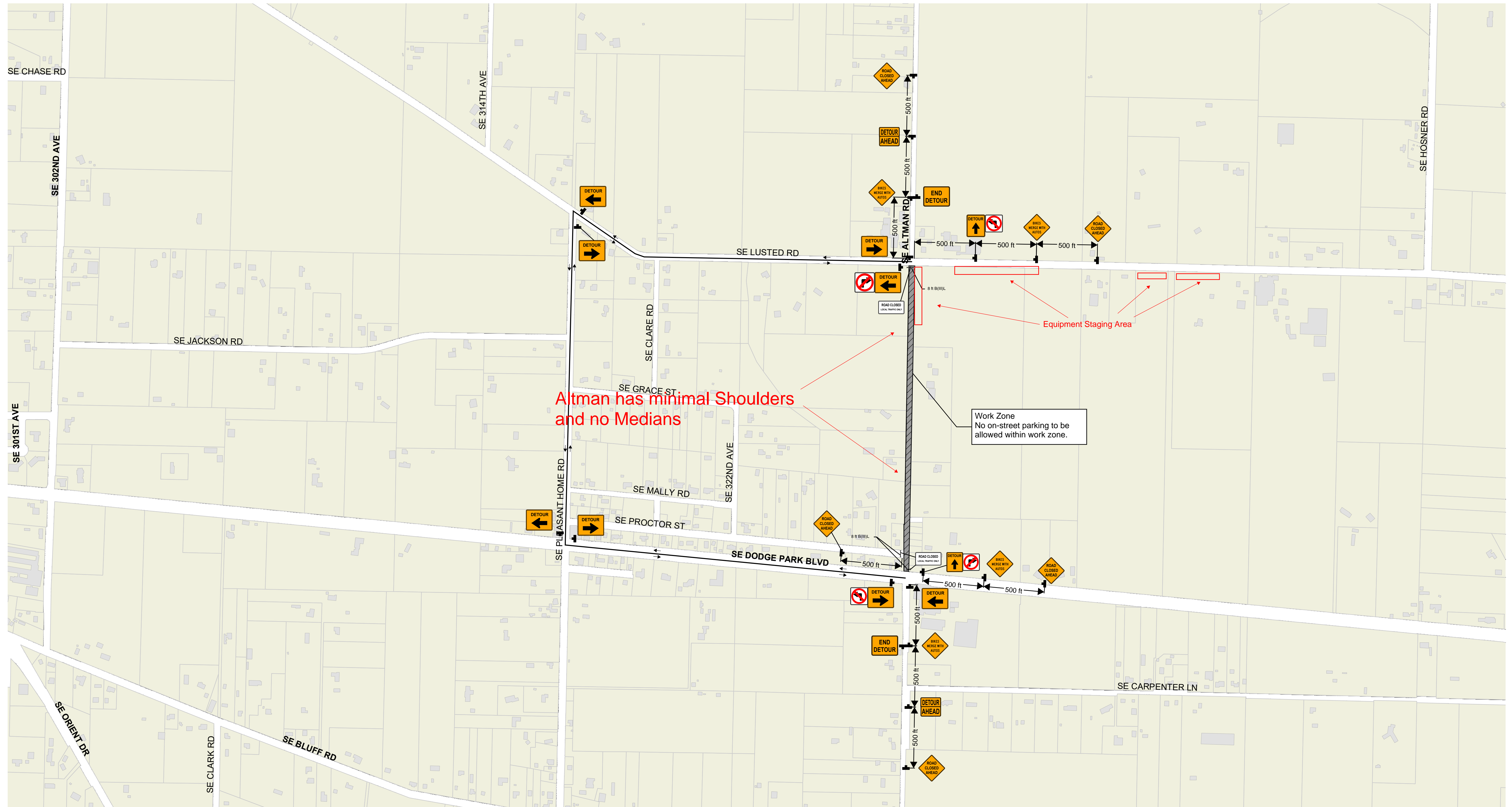
Kenneth M. Ackerman
Kenneth M. Ackerman, Principal Engineer, PE No 19424
Date: 8/19/24



Pavement Rehabilitation Project
SE Hosner Rd
From SE Lusted Rd
To SE Oxbow Dr
Plan Sheet
Sta 21+86C to Sta 26+13C

SAP Project No W02229
1/4 Section 3564, 3565 3664, 3665 Sheet No
24 of 24

North Altman Detour

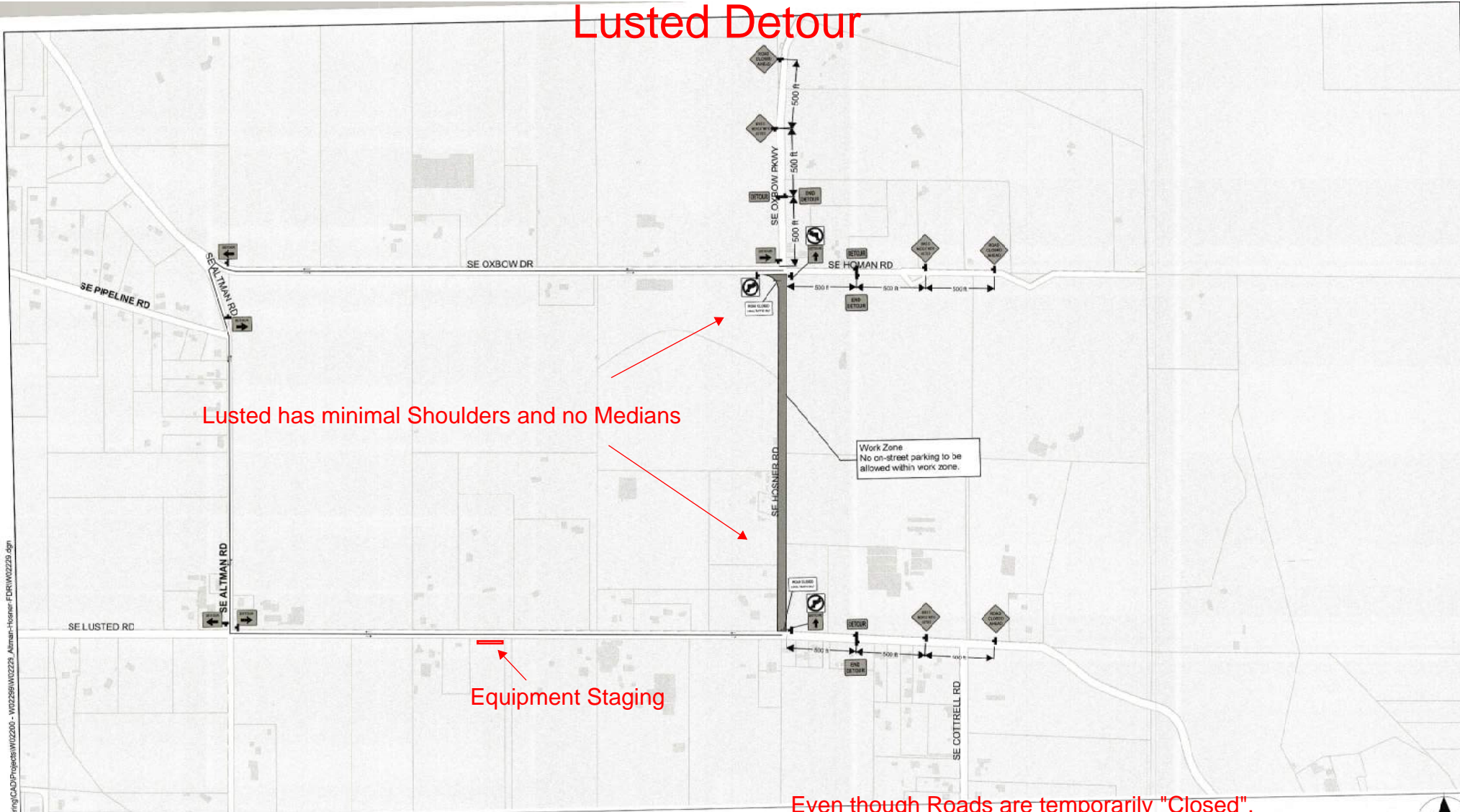


Emergency Contact:
Joe Armstrong 360-798-6093
Mike Logan 360-772-6351
McDonald Excavating, Inc.

Even though Roads are temporarily "Closed", one lane will always be open for Emergency Vehicles. We will stop work when Emergency Vehicles are coming through.



Lusted Detour



Lusted has minimal Shoulders and no Medians

Equipment Staging

Even though Roads are temporarily "Closed", one lane will always be open for Emergency Vehicles. We will stop work when Emergency Vehicles are coming through.

Emergency Contact:
 Joe Armstrong 360-798-6093
 Mike Logan 360-772-6351
 McDonald Excavating, Inc.

6/26/2024 8:55:51 AM J:\Engineering\CAO\Projects\W02229 - W02299\W02229 - Altman-Horner\FDR\W02229.dgn

No.	Date	Description	App'd.

Designed By BJK	Program Mgr DMC
CAD Checker By GVE	Const Mgr BJN
Checked By JDB	Const Super JDT
Project Mgr KMA	Date

POKPLAND WATER BUREAU
FROM FOREST TO FAUCET

Kenneth M. Adams
 Kenneth M. Adams, Principal Engineer, PE No 19424



Pavement Rehabilitation Project
SE Hosner Rd
 North from SE Lusted Rd
 Traffic Control Plan Sheet



SAP Project No
W02229
 1/4 Section
 3764, 3765, 3664,
 3665, 3564, 3565
 Sheet No

Lusted Detour



Emergency Contact:
Joe Armstrong 360-798-6093
Mike Logan 360-772-6351
McDonald Excavating, Inc.

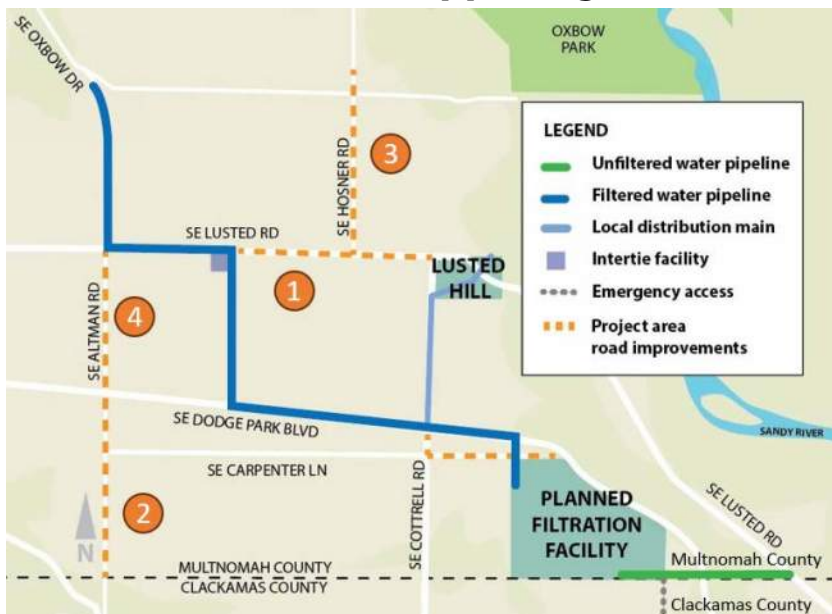
Even though Roads are temporarily "Closed", one lane will always be open for Emergency Vehicles. We will stop work when Emergency Vehicles are coming through.



Bull Run Heads Up! Road closures coming soon to your area

FILTRATION

Where work will be happening:



*Local and emergency access only during road closures. For detours and updates, visit www.portland.gov/FiltrationConstruction

What to expect:

- 1. Lusted Road**
 - 8/26-27 closed* 7a-7p
- 2. Altman Road (south of Dodge Park)**
 - 8/26 closed* 7a-7p
 - 8/27-29 closed* all day and night
 - 8/30 closed* 7a-7p
- 3. Hosner Road**
 - 8/28-29 closed* 7a-7p
- 4. Altman Road (north of Dodge Park)**
 - 8/28 closed* 7a-7p
 - 8/29-31 closed* until 8/31 5p
 - 8/31 5p -9/4 7a unpaved (sand) road open; 15 mph speed limit
 - 9/4 closed* for paving 7a-7p

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Bull Run TREATMENT PROJECTS

Filtration

The Portland Water Bureau is building a new water filtration facility and pipelines to make the Bull Run water supply safer and more resilient. Once online in September 2027, filtration treatment will protect public health and comply with safe drinking water regulations by removing *Cryptosporidium* and other potential contaminants from Bull Run water.



The City of Portland is committed to providing meaningful access. To request translation, interpretation, modifications, accommodations, or other auxiliary aids or services, contact 503-823-0102, Relay: 711.

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Письмовий і усний переклад | Traducere și interpretariat |
Chiaku me Awewen Kapas |

Translation and Interpretation: 503-823-0102 | portland.gov/water/access

Contact

503-823-1505

[bullrunprojects@
portlandoregon.gov](mailto:bullrunprojects@portlandoregon.gov)

Learn more

portland.gov/Filtration

[portland.gov/
FiltrationConstruction](http://portland.gov/FiltrationConstruction)

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- D. Developer shall make required improvements at the County Engineer's request if the transportation impact warrants additional road improvements. Such additional improvements shall not extend beyond the nearest intersection with the publicly maintained road. Improvements will be constructed in a manner consistent with the standards provided in the Design and Construction Manual.
- E. All costs relating to Local Access Road improvements shall be borne by the Developer including all administrative and other costs incurred by the County including but not limited to the oversight, review, inspection, etc, with respect to design, installation, and construction of any improvements on any Local Access Road under County jurisdiction. County shall not begin any work under this Section unless and until an adequate deposit as determined by the County Engineer has been received by the County to cover these costs.
- F. Notwithstanding any required improvements or other installations done in the public right of way of a Local Access Road under this Section 11 of these Rules, the County does not maintain such Local Access Road.

11.200 Naming: Naming or renaming of local access roads shall be done pursuant to MCC 11.05.500 through 11.05.575 or its successor.

12.000 Private Roads

12.100 Authority. Private roads are not subject to the authority of the County Transportation regulations and rules. Authority over private roads belongs to the land use and planning jurisdiction where the private road is located. However, should a proposal to alter or relocate the access point of a private road to a County Road or a Local Access Road, the requisite provisions of these Rules shall apply like any other road right of way access issue.

13.000 Temporary Road or Bridge Closures

13.100 When Applicable:

- A. A road or bridge is considered closed under this Section when all of its lanes are closed to through traffic for any extended period of time not less than one hour for the purposes of authorized work in the right of way as provided under these Rules.

- B. The following temporary bridge and road closures are not covered by or subject to the requirements of this Section 13.000, unless otherwise indicated:
 - 1. Temporary road closures for Special Events on County Roads are regulated under Section 21 of these Rules.
 - 2. Temporary bridge closure of a Willamette River Bridge as authorized and allowed under MCC 29.700-29.714 and 29.999.
 - 3. A temporary road or bridge closure initiated by the County at the direction of the County Engineer for maintenance or repair work or a public improvement project, including County initiated work because of emergency conditions; provided that for non-emergency temporary road or bridge closures the County shall follow the notice procedures set forth in Subsection 13.250.

13.200 *The Closure of Road or Bridge under County Jurisdiction:* Upon receiving a request in writing to close the County road or bridge to perform work in the right of way, the County Engineer shall consider the request for the temporary closure as provided in this Section 13. If the proposed roadwork is authorized under these Rules, the County Engineer will review any request for temporary closure subject to the following considerations:

- A. Traffic safety during the closure, which may be satisfied by a traffic control plan accepted by the County Engineer;
- B. Access and circulation for impacted properties in the area during the closure;
- C. Maintenance considerations during the closure;
- D. Undesirable effects on impacted properties and any other circumstances that can be documented resulting from the closure;
- E. Availability of reasonable alternatives to complete closure, if the closure would cause undue interference or hardship with the public's use of the road;
- D. Receipt of a sufficient deposit to cover the County's administrative costs and costs of posting or publishing notice as required under these Rules;

- F. Emergency road repair; and
- G. Appropriate review and coordination with all other affected governments including but not limited to any City, Tri-Met or the State of Oregon.

13.250 Notice of Intent to Close:

- A. The method of public notice required as a condition of a temporary road closure will be determined by the County Engineer based upon the length of closure, the traffic volume of the road to be closed, the availability of alternate routes and the anticipated impact of the closure on the surrounding transportation system. Possible public notice methods may include one or more of the following:
 - 1. Notice of temporary closure signs at each end of the road or road segment to be closed;
 - 2. Advertisement(s) in a newspaper of general circulation;
 - 3. Individual notices to abutting property owners by telephone, email or regular mail;
 - 4. Posting on the County website; or
 - 5. Through the County's Public Affairs Office.
- B. Both posted and published notices shall contain the following information:
 - 1. Description of the proposed action
 - 2. Date(s) of the proposed closure
 - 3. Request for comments to the County Engineer
 - 4. Address of the County Engineer
 - 5. Last date for accepting comments
 - 6. If applicable, the date the matter is to be heard by the County Board of Commissioners, if required under Section 13.600.

13.350 Physical Barriers: Shall be selected by the County Engineer to affect the closure giving consideration to aesthetics, safety, maintenance and economics.

13.400 Traffic Control Plan Required: The applicant or permittee shall submit a traffic control plan that conforms to the requirements set forth in the Design and Construction Manual for approval by the County Engineer. The road or bridge may not be closed until approval has been granted.

- 13.450 Requests for Closures: Requests for closure of twenty-four (24) hours or more must be submitted at least 14 days before the proposed closure. Public notice will be required for closures of more than twenty-four (24) hours. Requests for closures of twenty-four (24) hours or less may be authorized by the County Engineer without public notice.
- 13.500 Access to Property During Closures: To the extent feasible, access to property adjacent or abutting any temporary road closure must be maintained for occupants, patrons and emergency services.
- 13.600 Extended Temporary Closures, County Board Review; Procedures:
- A. An extended temporary closure is a road or bridge closure of thirty (30) days or more. Any request for an extended temporary closure of a Bridge, County Arterial or Collector is subject to the following procedures:
1. The applicant /permittee must submit a written request 60 days before the proposed extended closure to the County Engineer;
 2. The applicant/ permittee must post a deposit to cover the cost of the County's administrative costs and required public notice for submittal to the County Board;
 3. The County Engineer will review the request and associated traffic control plan for compliance with County standards;
 4. If after completion of the County Engineer's review the County Engineer deems the closure reasonable and finds that there are alternative routes and there is no undue hardship foreseen by the temporary closure for the requested time, the County Engineer will announce the proposed closure through one or more of the approved methods of notice set forth at Subsection 13.250;
 5. A party adversely affected by the proposed road or bridge closure under this Section may request to have a hearing before the County Board to oppose the proposed closure, provided such request is made in writing and received by the County Transportation Program or Bridge Shop within twenty one (21) calendar days of the date of the notice provided under Subsection 13.600 A.4;
 6. If the written request is timely received as provided herein, the Transportation Program or Bridge Shop as applicable, shall schedule the matter on the next available County Board Regular Meeting Agenda;

provided the County staff upon the submission of the request for hearing to the Board shall be under no further obligation to assist the party requesting the hearing, but may continue negotiation with the party to resolve any objections to the proposed closure prior to the scheduled hearing before the Board;

7. The Board's review is denovo and the Board shall review and consider the temporary closure request according to the same criteria as set forth in Section 13.200; and
8. The Board retains full discretion to approve, amend, postpone, remand to the County Engineer or reject the request.

- B. If the County Engineer denies a request for an extended temporary closure received under this Section, the party denied the closure may request a hearing before the County Board; any such hearing requested under this Subsection 13.600B. will be subject to the same timelines, procedures and conducted in the same manner as the hearing under Subsection 13.600A.

13.700 Duration of Temporary Closure. Any temporary closure of a road or bridge approved under this Section 13 shall not be longer than six (6) months.

14.000 Vacation of Right-of-Way

14.100 Except as provided herein all Vacation of Right-of-Way proceedings shall be conducted in a manner consistent with ORS 368.126, ORS 368.326 to 365.366 and the Multnomah County Code.

14.200 Preliminary Feasibility Study: An abutting property owner shall request the County Transportation Division prepare a Preliminary Feasibility Study (PFS) to evaluate a proposal for the vacation of County controlled right-of-way prior to submitting a petition vacation request under 14.400.

14.300 Fees, Timeline and Scope: A request for a PFS shall require a non-refundable fee as established by County Resolution. The County's timeline to prepare the PFS shall be flexible, dependent upon staff availability and resources to perform the task, but shall be produced no later than 45 days from the receipt of the required fee. The focus and scope of the PFS will be to assess whether the vacation serves the public interest and is consistent with all applicable State, Regional and Local plans for the County's planning and transportation needs. The PFS shall contain findings sufficient to